

PROJECT SPECIFICATIONS

# Tree Removal & Water Crossing

ST. CLAIR COUNTY, ILLINOIS

August 2022



ST. CLAIR COUNTY TRANSIT DISTRICT

## **NOTICE TO BIDDERS**

The St. Clair County Transit District, St. Clair County, Illinois, will receive sealed bids for **Tree Removal & Water Crossing** until **1:30 P.M.**, on **SEPTEMBER 12, 2022**, at the office of the St. Clair County Transit District. All bids will be publicly opened and read aloud at **1:35 P.M.**, on **SEPTEMBER 12, 2022**. **The sealed bids must be delivered to the St. Clair County Transit District office before 1:30 P.M., during regular business hours.** The Contract Documents, including Plans and Specifications, are on file at the office of the St. Clair County Transit District at 27 North Illinois Street, Belleville, Illinois, 62220.

The proposed project consists of the construction of a low water crossing and access roadway along with tree removal and grubbing at specified locations adjacent to proposed St. Clair Transit District facilities within MidAmerica Airport near Mascoutah, IL, and any other work necessary to complete the project as described. All work shall be performed in accordance with the Illinois Department of Transportation (IDOT) Standard Specifications for Road and Bridge Construction, edition in effect at the time of the bid.

IDOT Prequalification in Category 1 – Earth Excavation is required for all bidders.

No less than the prevailing rate of wages, as determined in accordance with the Illinois Prevailing Wage Act, shall be paid to all laborers, operators, teamsters, masons, plumbers, and any workers employed in construction on this project.

Each bidder must deposit with his bid, security in the amount of not less than five percent (5%) of the bidder's total bid price, such security being an acceptable bid bond, certified check, or cashier's check. The successful bidder will be required to furnish a satisfactory Performance Bond in the full amount of the bid.

Contractors must register their email with the St. Clair County Transit District by emailing Tony Erwin at [terwin@scctd.org](mailto:terwin@scctd.org). Bids will not be accepted or opened which have been received from any bidder who is not a bid document holder of record.

All questions shall be forwarded to Tony Erwin by email at [terwin@scctd.org](mailto:terwin@scctd.org).

**ST. CLAIR COUNTY TRANSIT DISTRICT**

# PROPOSAL

## Tree Removal & Water Crossing

### ST. CLAIR COUNTY TRANSIT DISTRICT

To: The Chairman and Board  
St. Clair County Transit District

The undersigned, having familiarized (himself/itself/themselves) with the local conditions affecting the cost of the work and with the Contract Documents, including the Notice to Bidders, General Conditions, Special Provisions, Specifications for Workmanship and Materials, Proposal, Form of Contract, Form of Bonds, etc., and with the plans and addenda on file in the Office of the St. Clair County Transit District, hereby proposes to perform all work required to be performed and to provide and furnish all of the labor, materials, necessary tools, expendable equipment, and all utility and transportation services necessary to perform and complete in a workmanlike manner all work required in connection with the **Tree Removal & Water Crossing** for the St. Clair County Transit District, all in accordance with the Plans and Specifications, including:

ADDENDA NUMBER

DATE

_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

issued thereto and acknowledged herein, for the unit prices given in the following **SCHEDULE OF QUANTITIES AND BID UNIT PRICES:**

# PROPOSAL

## Tree Removal & Water Crossing

### SCHEDULE OF QUANTITIES AND BID UNIT PRICES

Item	Description	Unit	Quantity	Unit Price	Total
1	Mobilization	L.S.	1		
2	Tree Removal	Acre	49.5		
3	Erosion Control	L.S.	1		
4	Low Flow Crossing	L.S.	1		
				Total	

# PROPOSAL

If awarded this contract, the undersigned agrees to commence work within ten (10) days after award of the contract or as otherwise directed, and to complete the work as outlined in the Completion Date special provision of the **Tree Removal & Water Crossing** specifications.

Accompanying this proposal is a (certified check), (cashier's check), (bid bond), in the amount of \_\_\_\_\_dollars (\$ \_\_\_\_\_), payable to the St. Clair County Transit District, which it is agreed will be forfeited to the St. Clair County Transit District if the undersigned fails to execute the contract.

\_\_\_\_\_  
BIDDER - COMPANY NAME

\_\_\_\_\_  
ADDRESS

\_\_\_\_\_  
CITY / STATE / ZIP

BY:  
\_\_\_\_\_

\_\_\_\_\_  
TITLE DATE

# PROPOSAL

## NON-COLLUSION AFFIDAVIT

The bidder, by its officers and their authorized agents or representatives present at the time of filing this bid, being duly sworn on their oaths, say that neither they nor any of them have in any way, directly or indirectly, entered into any arrangement or agreement with any other bidder, nor with any public officer of the ST. CLAIR COUNTY TRANSIT DISTRICT whereby such affiant or affiants, or either of them, has paid or is to pay to such other bidder or public officer any sum of money; nor has given or is to give such other bidder or public officer anything of value whatever, or such affiant or affiants; or either of them, has not, directly or indirectly, entered into any arrangement or agreement with any other bidder or bidders, which tends to or does lessen or destroy free competition in the letting of the Contract sought for by the attached bids; nor any inducement of any form or character, other than that which appears upon the face of the bid, will be suggested, offered, paid or delivered to any person whomsoever to influence the acceptance of the said bid or awarding of the Contract; nor has this bidder entered into any agreement or understanding of any kind whatsoever, with any person, in any way or manner, for any of the proceeds of the contract sought by this bid.

\_\_\_\_\_ BY: \_\_\_\_\_  
BIDDER - COMPANY NAME

\_\_\_\_\_ TITLE \_\_\_\_\_ DATE \_\_\_\_\_  
ADDRESS

\_\_\_\_\_  
CITY / STATE / ZIP

Subscribed and sworn to before me this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
NOTARY PUBLIC

My commission expires: \_\_\_\_\_

**NOTE:** Bidders should not add any conditions or qualifying statements to this bid, as otherwise the bid may be declared irregular as being not responsive to the Advertisement for Bids.

# PROPOSAL

## BID BOND

**WE,** \_\_\_\_\_, as PRINCIPAL, and \_\_\_\_\_, as SURETY with authority to do business in Illinois, are held and firmly bound unto the ST. CLAIR COUNTY TRANSIT DISTRICT in the penal sum of Five Percent (5%) of the total bid price lawful money of the United States. We bind ourselves jointly and severally, and our joint and several heirs, executors, administrators, successors, and assigns, firmly by these presents, this \_\_\_\_\_ day of \_\_\_\_\_, 2022, to pay to the ST. CLAIR COUNTY TRANSIT DISTRICT this sum under the conditions of this instrument.

**WHEREAS** the condition of the foregoing obligation is such that, the said PRINCIPAL is submitting a written proposal to the ST. CLAIR COUNTY TRANSIT DISTRICT for Tree Removal & Water Crossing.

**THEREFORE**, if the proposal is accepted and a contract awarded to the PRINCIPAL by the ST. CLAIR COUNTY TRANSIT DISTRICT for the aforementioned improvement, and the PRINCIPAL shall within ten (10) days after receipt of the notice of award enter into a formal contract and furnish evidence of the required insurance coverage, all as required by the project specifications, then this obligation shall become void; otherwise, it shall remain in full force and effect.

**IN THE EVENT** the ST. CLAIR COUNTY TRANSIT DISTRICT determines that the PRINCIPAL has failed to enter into a formal contract in compliance with any requirements set forth in the preceding paragraph, then the ST. CLAIR COUNTY TRANSIT DISTRICT shall immediately be entitled to recover the full penal sum set out above, together with all court costs, all attorney fees and any other expense of recovery.

**IN TESTIMONY WHEREOF**, the said PRINCIPAL and the said SURETY have caused this instrument to be signed by their respective officers this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

(SEAL) PRINCIPAL

\_\_\_\_\_  
PRINCIPAL - COMPANY NAME

\_\_\_\_\_  
ADDRESS

\_\_\_\_\_  
CITY / STATE / ZIP

ATTEST: \_\_\_\_\_

BY: \_\_\_\_\_

\_\_\_\_\_  
TITLE DATE

\_\_\_\_\_  
TITLE DATE

(SEAL) SURETY

\_\_\_\_\_  
SURETY - COMPANY NAME

BY:

\_\_\_\_\_  
ATTORNEY-IN-FACT DATE

Subscribed and sworn to before me this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
NOTARY PUBLIC

My commission expires: \_\_\_\_\_





# Check Sheet for Recurring Special Provisions



Local Public Agency	County	Section Number
St. Clair County Transit District	St. Clair	Tree Removal 2022

Check this box for lettings prior to 01/01/2022.

The Following Recurring Special Provisions Indicated By An "X" Are Applicable To This Contract And Are Included By Reference:

### Recurring Special Provisions

Check Sheet #		Reference Page No.
1	<input type="checkbox"/> Additional State Requirements for Federal-Aid Construction Contracts	1
2	<input type="checkbox"/> Subletting of Contracts (Federal-Aid Contracts)	4
3	<input type="checkbox"/> EEO	5
4	<input type="checkbox"/> Specific EEO Responsibilities Non Federal-Aid Contracts	15
5	<input type="checkbox"/> Required Provisions - State Contracts	20
6	<input type="checkbox"/> Asbestos Bearing Pad Removal	26
7	<input type="checkbox"/> Asbestos Waterproofing Membrane and Asbestos HMA Surface Removal	27
8	<input type="checkbox"/> Temporary Stream Crossings and In-Stream Work Pads	28
9	<input type="checkbox"/> Construction Layout Stakes	29
10	<input type="checkbox"/> Use of Geotextile Fabric for Railroad Crossing	32
11	<input type="checkbox"/> Subsealing of Concrete Pavements	34
12	<input type="checkbox"/> Hot-Mix Asphalt Surface Correction	38
13	<input type="checkbox"/> Pavement and Shoulder Resurfacing	40
14	<input type="checkbox"/> Patching with Hot-Mix Asphalt Overlay Removal	41
15	<input type="checkbox"/> Polymer Concrete	43
16	<input type="checkbox"/> PVC Pipeliner	45
17	<input type="checkbox"/> Bicycle Racks	46
18	<input type="checkbox"/> Temporary Portable Bridge Traffic Signals	48
19	<input type="checkbox"/> Nighttime Inspection of Roadway Lighting	50
20	<input type="checkbox"/> English Substitution of Metric Bolts	51
21	<input type="checkbox"/> Calcium Chloride Accelerator for Portland Cement Concrete	52
22	<input type="checkbox"/> Quality Control of Concrete Mixtures at the Plant	53
23	<input type="checkbox"/> Quality Control/Quality Assurance of Concrete Mixtures	61
24	<input type="checkbox"/> Digital Terrain Modeling for Earthwork Calculations	77
25	<input type="checkbox"/> Preventive Maintenance - Bituminous Surface Treatment (A-1)	79
26	<input type="checkbox"/> Temporary Raised Pavement Markers	85
27	<input type="checkbox"/> Restoring Bridge Approach Pavements Using High-Density Foam	86
28	<input type="checkbox"/> Portland Cement Concrete Inlay or Overlay	89
29	<input type="checkbox"/> Portland Cement Concrete Partial Depth Hot-Mix Asphalt Patching	93
30	<input type="checkbox"/> Longitudinal Joint and Crack Patching	96
31	<input type="checkbox"/> Concrete Mix Design - Department Provided	98
32	<input type="checkbox"/> Station Numbers in Pavements or Overlays	99

Local Public Agency

County

Section Number

St. Clair County Transit District

St. Clair

Tree Removal 2022

The Following Local Roads And Streets Recurring Special Provisions Indicated By An "X" Are Applicable To This Contract And Are Included By Reference:

Local Roads And Streets Recurring Special Provisions

<u>Check Sheet #</u>		<u>Page No.</u>
LRS 1	<b>Reserved</b>	101
LRS 2	<input type="checkbox"/> Furnished Excavation	102
LRS 3	<input checked="" type="checkbox"/> Work Zone Traffic Control Surveillance	103
LRS 4	<input type="checkbox"/> Flaggers in Work Zones	104
LRS 5	<input checked="" type="checkbox"/> Contract Claims	105
LRS 6	<input checked="" type="checkbox"/> Bidding Requirements and Conditions for Contract Proposals	106
LRS 7	<input type="checkbox"/> Bidding Requirements and Conditions for Material Proposals	112
LRS 8	<b>Reserved</b>	118
LRS 9	<input type="checkbox"/> Bituminous Surface Treatments	119
LRS 10	<b>Reserved</b>	123
LRS 11	<input checked="" type="checkbox"/> Employment Practices	124
LRS 12	<input checked="" type="checkbox"/> Wages of Employees on Public Works	126
LRS 13	<input checked="" type="checkbox"/> Selection of Labor	128
LRS 14	<input type="checkbox"/> Paving Brick and Concrete Paver Pavements and Sidewalks	129
LRS 15	<input checked="" type="checkbox"/> Partial Payments	132
LRS 16	<input type="checkbox"/> Protests on Local Lettings	133
LRS 17	<input checked="" type="checkbox"/> Substance Abuse Prevention Program	134
LRS 18	<input type="checkbox"/> Multigrade Cold Mix Asphalt	135
LRS 19	<input type="checkbox"/> Reflective Crack Control Treatment	136

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# **SPECIAL PROVISIONS**

## **Tree Removal & Water Crossing**

The following Special Provisions supplement the "Standard Specifications for Road and Bridge Construction", latest edition in effect on the date of the invitation for bids, as adopted by the Illinois Department of Transportation (IDOT) and the Supplemental Specifications and Recurring Special Provisions adopted therewith, and the "Illinois Manual on Uniform Traffic Control Devices for Streets and Highways" latest edition in effect on the date of the invitation for bids, shall apply to and govern the construction of the above designated project. In case of conflict with any part, or parts, of said specifications, these special provisions shall take precedence over the Standard Specifications and inserted special provisions.

All references to Department and Engineer in the "Standard Specifications for Road and Bridge Construction" shall be defined as the St. Clair County Transit District or their authorized representative.

**PROJECT LOCATION:** The project is located adjacent to proposed St. Clair County Transit District facilities on property within MidAmerica Airport near Mascoutah, Illinois.

**DESCRIPTION OF WORK:** The proposed project consists of the construction of a low water crossing and access roadway along with tree removal and grubbing at specified locations and any other work necessary to complete the project as described.

**GENERAL:** The Contractor shall furnish all labor, materials, and equipment to complete the work as described in accordance with these special provisions and the applicable sections of the IDOT Standard Specifications for Road and Bridge Construction.

**EXAMINATION OF SITE:** The Contractor shall be responsible for completing any necessary site inspections prior to submitting a bid on this project. Upon receipt of a bid, it shall be assumed that the Contractor is fully familiar with the project site and the work to be completed.

**PREQUALIFICATION:** The Contractor shall be prequalified through IDOT for work specified under Category 1 – Earth Excavation.

**CONTRACT ADMINISTRATOR:** The St. Clair County Transit District or its authorized representative will be the assigned Contract Administrator of this contract. This individual will be the point of contact for the Contractor regarding any issues or concerns relating to the contract. The Contract Administrator will also be responsible for scheduling site visits with the Contractor, reviewing sites for completion, enforcement of contract provisions and the receipt of invoices as provided by the Contractor. The Contractor will be provided contact information for the Contract Administrator upon award of the contract and will be subsequently notified should the administrator change during the life of the contract.

**PROJECT COORDINATION:** A mandatory pre-construction meeting shall be scheduled within one week of contract ratification between the Contractor and the St. Clair County Transit District. All work will be scheduled through the St. Clair County Transit District Director of Facilities. Contractor must present a preliminary work schedule at the pre-construction meeting. The St. Clair County Transit District will have progress meetings, if necessary, which shall be held at the St. Clair County Transit District Office.

**PERMITTING:** The Contractor shall be required to obtain any necessary permits to complete the project prior to beginning work.

**COOPERATION WITH CONTRACTORS/UTILITY COMPANIES:** Utility relocation and construction activities will be ongoing concurrently with the work proposed under this contract. The Contractor will cooperate fully with utility companies and their contractors in order to facilitate prompt completion of all work.

**COMPLETION DATE:** The Contractor will be issued notice to proceed upon receipt by the St. Clair County Transit District of various permits allowing from the U.S. Army Corps of Engineers (USACE). No work shall be completed until a respective notice to proceed is provided by the St. Clair County Transit District.

Crossing: Permit issuance for this work is anticipated to be received on or around 11/01/2022. All work shall be completed within 30 calendar days of receiving notice to proceed.

Area 1: Permit issuance for this work is anticipated to be received from USACE on or around 11/01/2022. Work in this area shall be completed within 30 calendar days of receiving notice to proceed.

Area 2: Permit issuance for this work is anticipated to be received from USACE on or around 11/01/2022. Work in this area shall be completed by 12/31/2022.

Area 3: Permit issuance for this work is anticipated to be received from USACE on or around 11/01/2022. Work in this area cannot begin prior to 11/01/2022 and shall be completed by 01/31/2023.

Area 4: Permit issuance for this work is anticipated to be received from USACE on or around 11/01/2022. In addition, work in the area cannot begin until a permit is issued by the Illinois Department of Transportation (IDOT). The permit from IDOT is expected to be received on or around 12/01/2022. Work in this area shall be completed by 01/31/2023.

Area 5: Permit issuance for this work is anticipated to be received from USACE on or around 12/01/2022. Work in this area shall be completed within 15 days of receiving notice to proceed.

**MIDAMERICA AIRPORT REQUIREMENTS:** Right-of-Entry will need to be obtained from MidAmerica Airport prior to beginning work activities. The Contractor shall complete and provide all necessary paperwork to:

Mr. Daniel J. Trapp, P.E.  
Director, Engineering & Planning  
MidAmerica St. Louis Airport (BLV)  
Phone (618) 566-5322  
Email [dan.trapp@flymidamerica.com](mailto:dan.trapp@flymidamerica.com)

**ILLINOIS DEPARTMENT OF TRANSPORTATION:** Prior to beginning work activities within IDOT right-of-way, the Contractor will be required to obtain authorization from the Illinois Department of Transportation. The Contractor shall complete all appropriate paperwork (BSPE Form 725, etc.) as needed by IDOT after their issuance of the permit.

**TREE REMOVAL:** This work shall consist of removal of all trees and clearing the areas as shown on the plan sheets. This work shall be done in accordance with Section 201 of the Standard Specifications, with the exception of Articles 201.05 (a) and 201.05 (b) which shall not apply.

**EROSION CONTROL:** This work shall consist of installation and maintenance of perimeter erosion barrier, temporary ditch checks, and temporary erosion control seeding as shown on the plan sheets and as discussed in this provision. This work shall be done in accordance with Section 280 of the Standard Specifications.

This work will be paid for at the LUMP SUM price for EROSION CONTROL.

**LOW FLOW CROSSING:** This work shall consist of constructing a low flow crossing and access roadway as detailed in the plans. Upon final acceptance, the Contractor will be required to maintain the low flow crossing and access roadway at the request of the St. Clair County Transit District until August 31, 2023, unless released in writing by the Transit District at an earlier date.

The low flow crossing and access roadway are for the future use of the St. Clair County Transit District. If the Contractor chooses to utilize the low flow crossing and access roadway during the tree removal and clearing operations, the Contractor shall restore the crossing and roadway to original condition upon the completion of work activities at no cost to the Transit District.

Construction of the low flow crossing and access roadway will be paid for at the LUMP SUM price for LOW FLOW CROSSING. Maintenance of the low flow crossing and access roadway, if requested by the St. Clair County Transit District, will be paid per Article 109.04 of the Standard Specifications.

**INVOICES:** The St. Clair County Transit District will pay all Contractor submitted invoices on a monthly basis. The St. Clair County Transit District will have 72 hours to inspect the project site before any submitted invoice is processed for payment. Under no circumstances will the Contract Administrator be authorized to pre-pay for work prior to completion or for the partial completion of work. Invoice templates shall follow the standard AIA form. The Contract Administrator can provide a template upon request. If the Contractor elects to use their own invoice, then said invoice shall include the following features: invoice number, invoice date, individual listing of project site with corresponding cost and account reference along with the date the work was completed. Invoices shall be emailed to the St. Clair County Transit District Office via email provided to the Contractor. All invoices submitted shall be paid **Net 30**.

**RETAINAGE:** To further assure timely completion of the work, restoration, and clean-up of the project site, 10% of the payment due to the Contractor shall be retained until the project is successfully completed and all punch list items are adequately addressed.

**PROJECT LABOR AGREEMENT (PLA) AND PREVAILING WAGES:** A PLA shall be required for this project and no less than the prevailing rate of wages, as determined in accordance with the Illinois Prevailing Wage Act, shall be paid to all laborers, operators, teamsters, masons, plumbers, and any workmen employed in the accomplishment of this project. On site wage interviews may be conducted periodically to assure compliance with the prevailing wage rates. **At the time of each payment request, the Contractor must submit copies of weekly payrolls as proof of compliance with prevailing wage rates. Payment will not be made without submittal of this documentation and full compliance with the prevailing wage rates.** For additional information, please visit the Illinois Department of Labor's Web Site at <https://www2.illinois.gov/idol>.

**TRAFFIC CONTROL PLAN:** Traffic control shall be in accordance with the applicable sections of the Standard Specifications for Road and Bridge Construction, the applicable guidelines contained in the National Manual on Uniform Traffic Control Devices for Streets and Highways, Illinois Supplement to the National Manual on Uniform Traffic Control Devices, these Special Provisions, and any special details and Highway Standards contained herein.

Special attention is called to Articles 107.09 and 107.14 of the Standard Specifications for Road and Bridge Construction and the following highway standards relating to traffic control:

- 701001      701006      701101      701106      701501      701901

In addition, the following Special Provision(s) will also govern traffic control for this project:

N/A

The Contractor shall install any additional traffic control and comply with all regulations required by MidAmerica Airport. Costs for compliance with these requirements will be included in the cost of the contract and no additional payment will be allowed.



**DISPOSAL OF MATERIALS:** All materials for disposal shall be removed from the site and properly disposed of at locations provided by the Contractor. The cost for doing this work shall be included in the bid prices and no additional compensation will be allowed.

**CONSTRUCTION LAYOUT:** The Contractor shall furnish and provide all construction layout and perform all work necessary to construct the work to the lines and grades shown on the plans. The Contractor will be provided adequate reference points to the centerline of survey or other control points as applicable, along with bench marks to be used during construction.

The Contractor shall provide field forces, equipment, and material to perform the entire layout for the work, set additional stakes, reference points, and any other horizontal or vertical controls, including supplementary bench marks, necessary to secure a correct layout of the work.

The Contractor shall be responsible for having the finished work conform to the lines, grades, elevations, and dimensions called for in the plans. Any inspection or checking of the Contractor's layout by the Transit District or their agent and the acceptance of all or any part of it shall not relieve the Contractor of his/her responsibility to secure the proper dimensions, grades, and elevations of the several parts of the work. The Contractor shall exercise care in the preservation of stakes and bench marks and shall have them reset when any are damaged, lost, displaced, removed, or otherwise obliterated.

Cost for this work shall be considered included in the costs of the various items of work being completed as part of the project and no additional compensation will be allowed.

**EQUIPMENT AND MATERIAL STORAGE:** The Contractor shall use staging locations as shown in the plans. No other sites shall be used by the Contractor without written approval of the St. Clair County Transit District.

**UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES:** It is the Contractor's responsibility to determine the existence and location of all underground facilities, structures, and utilities and to protect them from damage during construction.

**PROTECTION OF EXISTING FACILITIES:** The Contractor shall maintain and protect all existing facilities, both on-site and off-site, including but not limited to utilities, roadways, trees, tree limbs, landscaping, lawns, and buildings which are not scheduled for removal or replacement under this project. The Contractor shall provide any required sheeting, shoring, covering or other means and methods to protect existing facilities. The Contractor shall bear all costs and make all arrangements to repair or replace, any existing facility, both on-site and off-site, damaged as a result of construction, which is not scheduled for removal or replacement as part of this project.

**INDEMNIFICATION:** To the fullest extent permitted by law, the Contractor shall indemnify and hold harmless the St. Clair County Transit District, all commissioners of the St. Clair County Transit District Board, all commissioners of the St. Clair County Board, all commissioners of the St. Clair County Public Commission, MidAmerica St. Louis Airport, and agents and employees of any of them from and against all claims, damages, losses and expenses, direct, indirect or consequential (including but not limited to fees and charges of the St. Clair County Transit District's architects, attorneys and other professionals and court and arbitration costs) arising out of or resulting from the performance of the work, provided that any such claim, damage, loss or expense (a) is attributable to bodily injury, sickness, disease or death, or to injury to or destruction of tangible property (other than the work itself) including the loss of use resulting therefrom and (b) is caused in whole or in part by any negligent act or omission of the Contractor, any subcontractor, any person or organization directly or indirectly employed by any of them to perform or furnish any of the work or anyone for whose acts any of them may be liable, regardless of whether or not such claim, damage, loss or expense is caused in part by a party indemnified hereunder or arises by or is imposed by laws and regulations regardless of the negligence of any such party.

In any and all claims against the St. Clair County Transit District or any of their consultants, agents, or employees by any employee of the Contractor, any subcontractor, any person or organization directly or indirectly employed by any of them to perform or furnish any Work or anyone for whose acts any of them may be liable, the indemnification obligation under paragraph A shall not be limited in any way by any limitation on the amount or type of damages, compensation or benefits payable by or for the Contractor or any such subcontractor or other person or organization under workers' or workmen's compensation acts, disability benefit acts or other employee benefit acts.

The obligations of the Contractor shall not extend to the liability of the St. Clair County Transit District or their consultants, agents or employees arising out of the preparation or approval of maps, drawings, opinions, reports, surveys, change orders, designs, or specifications.

**SAFETY AND PROTECTION:** The Contractor shall be responsible for initiating, maintaining, and supervising all safety precautions and programs in connection with the project. The Contractor shall take all necessary precautions for the safety of, and shall provide the necessary protection to prevent damage, injury, or loss to:

- All employees on the project and other persons and organizations who may be affected thereby; and
- All work, materials, and equipment to be incorporated therein, whether in storage on or off the site; and
- Other property at the site adjacent thereto, including trees, shrubs, lawns, walks, pavements, roadways, structures, utilities, and underground facilities not designated for removal, relocation, or replacement in the course of construction.

The Contractor shall comply with all applicable Laws and Regulations of any public body having jurisdiction for the safety of persons and property or to protect them from damage,

injury or loss; and shall erect and maintain all necessary safeguards for such safety and protection. The Contractor shall notify owners of adjacent property and of underground facilities and utility owners when prosecution of the work may affect them and shall cooperate with them in the protection, removal, relocation, and replacement of their property. All damage, injury or loss to any property caused, directly or indirectly, in whole or in part, by the Contractor, any subcontractor, supplier or any other person or organization directly or indirectly employed by any of them to perform or furnish any of the work or anyone for whose acts either of them may be liable, shall be remedied by the Contractor (except damage or loss attributable to the fault of drawings or specifications or to the acts or omissions of the St. Clair County Transit District or anyone employed therein or anyone for whose acts either of them may be liable, and not attributable, directly or indirectly, in whole or in part, to the fault or negligence of the Contractor). The Contractor's duties and responsibilities for the safety and protection of the work shall continue until such time as all the Work is completed and the St. Clair County Transit District has issued a notice to the Contractor that the work is acceptable (except as otherwise expressly provided in connection with Substantial Completion).

The Contractor shall designate a responsible representative at the site whose duty shall be the prevention of accidents. This person shall be the Contractor's superintendent, unless otherwise designated in writing by the Contractor to the St. Clair County Transit District.

In emergencies affecting the safety or protection of persons or the work or property at the site or adjacent thereto, the Contractor, without special instructions or authorization from the St. Clair County Transit District, is obligated to act to prevent threatened damage, injury, or loss. The Contractor shall give the St. Clair County Transit District prompt, written notice if the Contractor believes that any significant changes in the work or variations from the contract documents have been caused thereby. If the St. Clair County Transit District determines that a change in the contract documents is required because of the action taken in response to an emergency, a work directive change or change order will be issued to document the consequences of the changes or variations.

**CONFLICTS WITH PERSONNEL:** Should a conflict between personnel of the Contractor and the St. Clair County Transit District escalate to the point that it hinders progress of the work and cannot be settled amicably, the Contractor personnel involved in the conflict shall be removed from the project. A personnel conflict shall not give cause for the Contractor to terminate this contract nor to pull employees from the active work site.

## **COMPENSABLE DELAY COSTS (BDE)**

Effective: June 2, 2017

Revised: April 1, 2019

Revise Article 107.40(b) of the Standard Specifications to read:

“(b) Compensation. Compensation will not be allowed for delays, inconveniences, or damages sustained by the Contractor from conflicts with facilities not meeting the above definition; or if a conflict with a utility in an unanticipated location does not cause a shutdown of the work or a documentable reduction in the rate of progress exceeding the limits set herein. The provisions of Article 104.03 notwithstanding, compensation for delays caused by a utility in an unanticipated location will be paid according to the provisions of this Article governing minor and major delays or reduced rate of production which are defined as follows.

- (1) Minor Delay. A minor delay occurs when the work in conflict with the utility in an unanticipated location is completely stopped for more than two hours, but not to exceed two weeks.
- (2) Major Delay. A major delay occurs when the work in conflict with the utility in an unanticipated location is completely stopped for more than two weeks.
- (3) Reduced Rate of Production Delay. A reduced rate of production delay occurs when the rate of production on the work in conflict with the utility in an unanticipated location decreases by more than 25 percent and lasts longer than seven calendar days.”

Revise Article 107.40(c) of the Standard Specifications to read:

“(c) Payment. Payment for Minor, Major, and Reduced Rate of Production Delays will be made as follows.

- (1) Minor Delay. Labor idled which cannot be used on other work will be paid for according to Article 109.04(b)(1) and (2) for the time between start of the delay and the minimum remaining hours in the work shift required by the prevailing practice in the area.

Equipment idled which cannot be used on other work, and which is authorized to standby on the project site by the Engineer, will be paid for according to Article 109.04(b)(4).

- (2) Major Delay. Labor will be the same as for a minor delay.

Equipment will be the same as for a minor delay, except Contractor-owned equipment will be limited to two weeks plus the cost of move-out to either the

Contractor's yard or another job and the cost to re-mobilize, whichever is less. Rental equipment may be paid for longer than two weeks provided the Contractor presents adequate support to the Department (including lease agreement) to show retaining equipment on the job is the most economical course to follow and in the public interest.

- (3) Reduced Rate of Production Delay. The Contractor will be compensated for the reduced productivity for labor and equipment time in excess of the 25 percent threshold for that portion of the delay in excess of seven calendar days. Determination of compensation will be in accordance with Article 104.02, except labor and material additives will not be permitted.

Payment for escalated material costs, escalated labor costs, extended project overhead, and extended traffic control will be determined according to Article 109.13.”

Revise Article 108.04(b) of the Standard Specifications to read:

“(b) No working day will be charged under the following conditions.

- (1) When adverse weather prevents work on the controlling item.
- (2) When job conditions due to recent weather prevent work on the controlling item.
- (3) When conduct or lack of conduct by the Department or its consultants, representatives, officers, agents, or employees; delay by the Department in making the site available; or delay in furnishing any items required to be furnished to the Contractor by the Department prevents work on the controlling item.
- (4) When delays caused by utility or railroad adjustments prevent work on the controlling item.
- (5) When strikes, lock-outs, extraordinary delays in transportation, or inability to procure critical materials prevent work on the controlling item, as long as these delays are not due to any fault of the Contractor.
- (6) When any condition over which the Contractor has no control prevents work on the controlling item.”

Revise Article 109.09(f) of the Standard Specifications to read:

“(f) Basis of Payment. After resolution of a claim in favor of the Contractor, any adjustment in time required for the work will be made according to Section 108. Any adjustment in the costs to be paid will be made for direct labor, direct materials, direct equipment, direct jobsite overhead, direct offsite overhead, and other direct costs allowed by the resolution. Adjustments in costs will not be made for interest charges, loss of anticipated profit, undocumented loss of efficiency, home office overhead and unabsorbed overhead

other than as allowed by Article 109.13, lost opportunity, preparation of claim expenses and other consequential indirect costs regardless of method of calculation.

The above Basis of Payment is an essential element of the contract and the claim cost recovery of the Contractor shall be so limited.”

Add the following to Section 109 of the Standard Specifications.

**“109.13 Payment for Contract Delay.** Compensation for escalated material costs, escalated labor costs, extended project overhead, and extended traffic control will be allowed when such costs result from a delay meeting the criteria in the following table.

Contract Type	Cause of Delay	Length of Delay
Working Days	Article 108.04(b)(3) or Article 108.04(b)(4)	No working days have been charged for two consecutive weeks.
Completion Date	Article 108.08(b)(1) or Article 108.08(b)(7)	The Contractor has been granted a minimum two week extension of contract time, according to Article 108.08.

Payment for each of the various costs will be according to the following.

- (a) Escalated Material and/or Labor Costs. When the delay causes work, which would have otherwise been completed, to be done after material and/or labor costs have increased, such increases will be paid. Payment for escalated material costs will be limited to the increased costs substantiated by documentation furnished by the Contractor. Payment for escalated labor costs will be limited to those items in Article 109.04(b)(1) and (2), except the 35 percent and 10 percent additives will not be permitted.
- (b) Extended Project Overhead. For the duration of the delay, payment for extended project overhead will be paid as follows.
  - (1) Direct Jobsite and Offsite Overhead. Payment for documented direct jobsite overhead and documented direct offsite overhead, including onsite supervisory and administrative personnel, will be allowed according to the following table.

Original Contract Amount	Supervisory and Administrative Personnel
Up to \$5,000,000	One Project Superintendent
Over \$ 5,000,000 - up to \$25,000,000	One Project Manager, One Project Superintendent or Engineer, and One Clerk
Over \$25,000,000 - up to \$50,000,000	One Project Manager, One Project Superintendent, One Engineer, and

	One Clerk
Over \$50,000,000	One Project Manager, Two Project Superintendents, One Engineer, and One Clerk

(2) Home Office and Unabsorbed Overhead. Payment for home office and unabsorbed overhead will be calculated as 8 percent of the total delay cost.

(c) Extended Traffic Control. Traffic control required for an extended period of time due to the delay will be paid for according to Article 109.04.

When an extended traffic control adjustment is paid under this provision, an adjusted unit price as provided for in Article 701.20(a) for increase or decrease in the value of work by more than ten percent will not be paid.

Upon payment for a contract delay under this provision, the Contractor shall assign subrogation rights to the Department for the Department's efforts of recovery from any other party for monies paid by the Department as a result of any claim under this provision. The Contractor shall fully cooperate with the Department in its efforts to recover from another party any money paid to the Contractor for delay damages under this provision."



## CONSTRUCTION AIR QUALITY – DIESEL RETROFIT (BDE)

Effective: June 1, 2010

Revised: November 1, 2014

The reduction of emissions of particulate matter (PM) for off-road equipment shall be accomplished by installing retrofit emission control devices. The term “equipment” refers to diesel fuel powered devices rated at 50 hp and above, to be used on the jobsite in excess of seven calendar days over the course of the construction period on the jobsite (including rental equipment).

Contractor and subcontractor diesel powered off-road equipment assigned to the contract shall be retrofitted using the phased in approach shown below. Equipment that is of a model year older than the year given for that equipment’s respective horsepower range shall be retrofitted:

Effective Dates	Horsepower Range	Model Year
June 1, 2010 <sup>1/</sup>	600-749	2002
	750 and up	2006
June 1, 2011 <sup>2/</sup>	100-299	2003
	300-599	2001
	600-749	2002
	750 and up	2006
June 1, 2012 <sup>2/</sup>	50-99	2004
	100-299	2003
	300-599	2001
	600-749	2002
	750 and up	2006

1/ Effective dates apply to Contractor diesel powered off-road equipment assigned to the contract.

2/ Effective dates apply to Contractor and subcontractor diesel powered off-road equipment assigned to the contract.

The retrofit emission control devices shall achieve a minimum PM emission reduction of 50 percent and shall be:

- a) Included on the U.S. Environmental Protection Agency (USEPA) *Verified Retrofit Technology List* (<http://www.epa.gov/cleandiesel/verification/verif-list.htm>), or verified by the California Air Resources Board (CARB) (<http://www.arb.ca.gov/diesel/verdev/vt/cvt.htm>); or
- b) Retrofitted with a non-verified diesel retrofit emission control device if verified retrofit emission control devices are not available for equipment proposed to be used on the project, and if the Contractor has obtained a performance certification from the retrofit



device manufacturer that the emission control device provides a minimum PM emission reduction of 50 percent.

Note: Large cranes (Crawler mounted cranes) which are responsible for critical lift operations are exempt from installing retrofit emission control devices if such devices adversely affect equipment operation.

Diesel powered off-road equipment with engine ratings of 50 hp and above, which are unable to be retrofitted with verified emission control devices or if performance certifications are not available which will achieve a minimum 50 percent PM reduction, may be granted a waiver by the Department if documentation is provided showing good faith efforts were made by the Contractor to retrofit the equipment.

Construction shall not proceed until the Contractor submits a certified list of the diesel powered off-road equipment that will be used, and as necessary, retrofitted with emission control devices. The list(s) shall include (1) the equipment number, type, make, Contractor/rental company name; and (2) the emission control devices make, model, USEPA or CARB verification number, or performance certification from the retrofit device manufacturer. Equipment reported as fitted with emissions control devices shall be made available to the Engineer for visual inspection of the device installation, prior to being used on the jobsite.

The Contractor shall submit an updated list of retrofitted off-road construction equipment as retrofitted equipment changes or comes on to the jobsite. The addition or deletion of any diesel powered equipment shall be included on the updated list.

If any diesel powered off-road equipment is found to be in non-compliance with any portion of this special provision, the Engineer will issue the Contractor a diesel retrofit deficiency deduction.

Any costs associated with retrofitting any diesel powered off-road equipment with emission control devices shall be considered as included in the contract unit prices bid for the various items of work involved and no additional compensation will be allowed. The Contractor's compliance with this notice and any associated regulations shall not be grounds for a claim.

### **Diesel Retrofit Deficiency Deduction**

When the Engineer determines that a diesel retrofit deficiency exists, a daily monetary deduction will be imposed for each calendar day or fraction thereof the deficiency continues to exist. The calendar day(s) will begin when the time period for correction is exceeded and end with the Engineer's written acceptance of the correction. The daily monetary deduction will be \$1,000.00 for each deficiency identified.

The deficiency will be based on lack of diesel retrofit emissions control.

If a Contractor accumulates three diesel retrofit deficiency deductions for the same piece of equipment in a contract period, the Contractor will be shutdown until the deficiency is corrected.

Such a shutdown will not be grounds for any extension of the contract time, waiver of penalties, or be grounds for any claim.

80261

## **SUBCONTRACTOR MOBILIZATION PAYMENTS (BDE)**

Effective: November 2, 2017

Revised: April 1, 2019

Replace the second paragraph of Article 109.12 of the Standard Specifications with the following:

“This mobilization payment shall be made at least seven days prior to the subcontractor starting work. The amount paid shall be at the following percentage of the amount of the subcontract reported on form BC 260A submitted for the approval of the subcontractor’s work.

Value of Subcontract Reported on Form BC 260A	Mobilization Percentage
Less than \$10,000	25%
\$10,000 to less than \$20,000	20%
\$20,000 to less than \$40,000	18%
\$40,000 to less than \$60,000	16%
\$60,000 to less than \$80,000	14%
\$80,000 to less than \$100,000	12%
\$100,000 to less than \$250,000	10%
\$250,000 to less than \$500,000	9%
\$500,000 to \$750,000	8%
Over \$750,000	7%”

80391

State of Illinois  
Department of Transportation  
Bureau of Local Roads and Streets

SPECIAL PROVISION  
FOR  
INSURANCE

Effective: February 1, 2007  
Revised: August 1, 2007

All references to Sections or Articles in this specification shall be construed to mean specific Section or Article of the Standard Specifications for Road and Bridge Construction, adopted by the Department of Transportation.

The Contractor shall name the following entities as additional insured under the Contractor's general liability insurance policy in accordance with Article 107.27:

St. Clair County Transit District

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St. Clair County and the St. Clair County Board

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St. Clair County Public Building Commission

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MidAmerica St. Louis Airport

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Gonzalez Companies, LLC

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The entities listed above and their officers, employees, and agents shall be indemnified and held harmless in accordance with Article 107.26.



Storm Water Pollution Prevention Plan



Route N/A	Marked Route N/A	Section Number Tree Removal and Water Crossing
Project Number N/A	County St. Clair	Contract Number N/A

This plan has been prepared to comply with the provisions of the National Pollutant Discharge Elimination System (NPDES) Permit No. ILR10 (Permit ILR10), issued by the Illinois Environmental Protection Agency (IEPA) for storm water discharges from construction site activities.

I certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations.

Signature	Date

Print Name Ken Sharkey	Title Managing Director	Agency St. Clair County Transit District
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Note: Guidance on preparing each section of BDE 2342 can be found in Chapter 41 of the IDOT Bureau of Design and Environment (BDE) Manual. Chapter 41 and this form also reference the IDOT Drainage Manual which should be readily available.

I. Site Description:

A. Provide a description of the project location; include latitude and longitude, section, town, and range:

The project is located to the west and north of Scott AFB and north of Mid-America Airport (MAA) near the Village of Shiloh located in St. Clair County in Sections 1, 2, 3, 4, 9, 12, & 16 in Township 1N in Range 7W and Sections 6 & 7 in Township 1N in Range 6W. The project limits are from the St. Clair County Metrolink Shiloh-Scott Station (west side of Scott AFB) to north of MidAmerica Airport, south of I-64, approximately 5.5 miles in length. The general location at the Shiloh-Scott Station is: 38° 32' 21" by 89° 52' 44" and the general location at the end of the project is: 38° 32' 55" by 89° 48' 45". This contract package is titled: Tree Removal and Water Crossing.

B. Provide a description of the construction activity which is the subject of this plan. Include the number of construction stages, drainage improvements, in-stream work, installation, maintenance, removal of erosion measures, and permanent stabilization:

The proposed project consists of approximately 48 acres of tree removal and construction of a water crossing associated with the future construction of utility relocations, light rail railroad, bike trail, and access roadway. Temporary erosion control will be included for all disturbed areas.

C. Provide the estimated duration of this project:

5 Months

D. The total area of the construction site is estimated to be 48 acres.

The total area of the site estimated to be disturbed by excavation, grading or other activities is 48 acres.

E. The following are weighted averages of the runoff coefficient for this project before and after construction activities are completed; see Section 4-102 of the IDOT Drainage Manual:

Before = 0.32 & After = 0.44

F. List all soils found within project boundaries; include map unit name, slope information, and erosivity:

Downsouth silt loam, 2 to 5 percent slopes  
Wakeland silt loam, 0 to 2 percent slopes, frequently flooded  
Edwardsville silt loam, 0 to 2 percent slopes  
Mascoutah silty clay loam, 0 to 2 percent slopes  
Winfield silt loam, 2 to 5 percent slopes  
Winfield silt loam, 5 to 10 percent slopes, eroded  
Menfro silt loam, 2 to 5 percent slopes  
Menfro silt loam, 5 to 10 percent slopes, eroded  
Bethalto silt loam, 0 to 2 percent slopes  
Birds silt loam, undrained, 0 to 2 percent slopes, frequently flooded  
Homen silt loam, 2 to 5 percent slopes  
Homen silt loam, 2 to 5 percent slopes, eroded  
Homen silt loam, 5 to 10 percent slopes, eroded  
Marine silt loam, 0 to 2 percent slopes  
Menfro silt loam, 2 to 5 percent slopes  
Menfro silt loam, 5 to 10 percent slopes, eroded  
Oconee silt loam, 0 to 2 percent slopes  
Pierron silt loam, 0 to 2 percent slopes  
Ruma silty clay loam, 2 to 5 percent slopes, eroded  
Ruma silty clay loam, 5 to 10 percent slopes, severely eroded  
Ruma silty clay loam, 10 to 18 percent slopes, severely eroded  
Wakeland silt loam, 0 to 2 percent slopes, frequently flooded  
Winfield silt loam, 2 to 5 percent slopes  
Winfield silt loam, 5 to 10 percent slopes, eroded

G. If wetlands were delineated for this project, provide an extent of wetland acreage at the site; see Phase I report:

Approximately 5.52 acres of emergent wetland and 5.39 acres of forested wetland will be directly impacted by the proposed project. Proposed discharges of dredged or fill material to wetlands is being coordinated through USACE. Impacts are anticipated to be permitted through the receipt of an individual Section 404 permit.

H. Provide a description of potentially erosive areas associated with this project:

The soil map as developed by the Natural Resources Conservation Services for this section of St. Clair County was utilized to identify the potentially erosive soils with the proposed development. The St. Clair County soils map indicates that soils within the project area have a 'slight' to 'severe' potential for erosion. The erosion control plan provides Best Management Practices (BMPs) to minimize erosion from occurring during construction.

I. The following is a description of soil disturbing activities by stages, their locations, and their erosive factors (e.g., steepness of slopes, length of slopes, etc.):

See plan sheets for locations of construction activities and soil disturbance.

J. See the erosion control plans and/or drainage plans for this contract for information regarding drainage patterns, approximate slopes anticipated before and after major grading activities, locations where vehicles enter or exit the site and controls to prevent offsite sediment tracking (to be added after contractor identifies locations), areas of soil disturbance, the location of major structural and non-structural controls identified in the plan, the location of areas where stabilization practices are expected to occur, surface waters (including wetlands) , and locations where storm water is discharged to surface water including wetlands.

K. Identify who owns the drainage system (municipality or agency) this project will drain into:

Waters of the United States

L. The following is a list of General NPDES ILR40 permittees within whose reporting jurisdiction this project is located:

St. Clair County MS4 Group is a co-permittee group which includes the following adjacent communities to the

project within a portion of the Lower Silver Creek watershed: St. Clair County, City of Belleville, City of Fairview Heights, City of O'Fallon, Village of Shiloh, Caseyville Township, O'Fallon Township, & St. Clair Township.

M. The following is a list of receiving water(s) and the ultimate receiving water(s) for this site. In addition, include receiving waters that are listed as Biologically Significant Streams by the Illinois Department of Natural Resources (IDNR). The location of the receiving waters can be found on the erosion and sediment control plans:

Direct receiving waters include Ash Creek, unnamed tributaries to Ash Creek, Silver Creek, and unnamed tributaries to Silver Creek. Ultimate receiving waters include Silver Creek, as well as Loop Creek, which itself is a tributary to Silver Creek. Silver Creek ultimately drains to the Kaskaskia River which ultimately drains to the Mississippi River. None of these direct receiving waters are listed as Biologically Significant Streams.

N. Describe areas of the site that are to be protected or remain undisturbed. These areas may include steep slopes (i.e., 1:3 or steeper), highly erodible soils, streams, stream buffers, specimen trees, natural vegetation, nature preserves, etc. Include any commitments or requirements to protect adjacent wetlands.

For any storm water discharges from construction activities within 50-feet of Waters of the U.S. (except for activities for water-dependent structures authorized by a Section 404 permit, describe: a) How a 50-foot undisturbed natural buffer will be provided between the construction activity and the Waters of the U.S. or b) How additional erosion and sediment controls will be provided within that area.

There are no areas that require special soil protection and no trees are identified for protection.

O. Per the Phase I document, the following sensitive environmental resources are associated with this project and may have the potential to be impacted by the proposed development. Further guidance on these resources is available in Section 41-4 of the BDE Manual.

Wetlands, Streams, and associated Riparian areas will be impacted by the proposed project. Several riparian and other wooded areas were identified as potential habitat for threatened and endangered bat species.

303(d) Listed receiving waters for suspended solids, turbidity, or siltation.  
The name(s) of the listed water body, and identification of all pollutants causing impairment:

There are no 303(d) Listed waters within the proposed project boundaries. The nearest 303(d) Listed impaired water for sediment/siltation that may receive storm water runoff from the proposed project is Loop Creek, which is located approximately 3.8 miles downstream at its nearest confluence with Ash Creek.

Provide a description of how erosion and sediment control practices will prevent a discharge of sediment resulting from a storm event equal to or greater than a twenty-five (25) year, twenty-four (24) hour rainfall event:

Maintaining the listed practices in this plan will not increase discharge levels of sediment.

Provide a description of the location(s) of direct discharge from the project site to the 303(d) water body:

There are no direct discharge points from this project.

Provide a description of the location(s) of any dewatering discharges to the MS4 and/or water body:

There are no dewatering discharges from this project.

Applicable Federal, Tribal, State, or Local Programs

There are no other applicable Federal or Tribal soil and erosion control and storm water management requirements that apply to the project. However, St. Clair County MS4 General NPDES ILR40 and IDNR Section 401 WQC requirements may apply.

Floodplain

The proposed project boundaries are located within a mapped FEMA-designated floodplain. The proposed project extends through Special Flood Hazard Area Zone X, A, and AE. A state floodplain permit issued by the IDNR Office of Water Resources and a county permit issued by St. Clair County Planning and Zoning is anticipated to be required.

Historic Preservation

Several cultural resource sites were identified and delineated within the proposed project boundaries. Consultation with USACE, IHPA, and Tribes is ongoing.

Receiving waters with Total Maximum Daily Load (TMDL) for sediment, total suspended solids, turbidity or siltation TMDL (fill out this section if checked above)

The name(s) of the listed water body:

The nearest 303(d) Listed impaired water for sediment/siltation that may receive storm water runoff from the proposed project is Loop Creek, which is located approximately 3.8 miles downstream at its nearest confluence with Ash Creek. No TMDLs have been developed for this segment of Loop Creek.

Provide a description of the erosion and sediment control strategy that will be incorporated into the site design that is consistent with the assumptions and requirements of the TMDL:

There are no direct discharges into Loop Creek. Ditch checks and perimeter barrier will be used in conjunction with establishing vegetative growth to the disturbed earth as soon as possible to prevent any discharges towards the receiving waters.

If a specific numeric waste load allocation has been established that would apply to the project's discharges, provide a description of the necessary steps to meet that allocation:

Threatened and Endangered Species/Illinois Natural Areas (INAI)/Nature Preserves

No threatened or endangered species, INAI areas, or Nature Preserves were identified within the project boundaries. However, potential summer roosting habitat for federal- and state-listed endangered Indiana bat and federal-listed threatened northern long-eared bat were identified.

Other

Wetland

Wetlands were identified and delineated within the proposed project boundaries. Proposed discharges of dredged or fill material to wetlands is being coordinated through USACE. Impacts are anticipated to be permitted through the receipt of an individual Section 404 permit, and will be mitigated through the purchase of wetland credits from an approved mitigation bank.

P. The following pollutants of concern will be associated with this construction project:

- |   |  |
|---|--|
| <input type="checkbox"/> Antifreeze / Coolants  | <input type="checkbox"/> Solid Waste Debris                                |
| <input type="checkbox"/> Concrete   | <input type="checkbox"/> Solvents  |
| <input type="checkbox"/> Concrete Curing Compounds                                      | <input type="checkbox"/> Waste water from cleaning construction equipments |
| <input type="checkbox"/> Concrete Truck Waste   | <input type="checkbox"/> Other (Specify) _____                             |
| <input checked="" type="checkbox"/> Fertilizers / Pesticides                            | <input type="checkbox"/> Other (Specify) _____                             |
| <input type="checkbox"/> Paints   | <input type="checkbox"/> Other (Specify) _____                             |
| <input type="checkbox"/> Petroleum (gas, diesel, oil, kerosene, hydraulic oil / fluids) | <input type="checkbox"/> Other (Specify) _____                             |
| <input checked="" type="checkbox"/> Soil Sediment                                       | <input type="checkbox"/> Other (Specify) _____                             |

**II. Controls:**

This section of the plan addresses the controls that will be implemented for each of the major construction activities described in Section I.C above and for all use areas, borrow sites, and waste sites. For each measure discussed, the Contractor will be responsible for its implementation as indicated. The Contractor shall provide to the Resident Engineer a plan for the implementation of the measures indicated. The Contractor, and subcontractors, will notify the Resident Engineer of any proposed changes, maintenance, or modifications to keep construction activities compliant with the Permit ILR10. Each such Contractor has signed the required certification on forms which are attached to, and are a part of, this plan:

A. **Erosion and Sediment Controls:** At a minimum, controls must be coordinated, installed and maintained to:

1. Minimize the amount of soil exposed during construction activity;
2. Minimize the disturbance of steep slopes;
3. Maintain natural buffers around surface waters, direct storm water to vegetated areas to increase sediment removal and maximize storm water infiltration, unless infeasible;
4. Minimize soil compaction and, unless infeasible, preserve topsoil.



**B. Stabilization Practices:** Provided below is a description of interim and permanent stabilization practices, including site- specific scheduling of the implementation of the practices. Site plans will ensure that existing vegetation is preserved where attainable and disturbed portions of the site will be stabilized. Stabilization practices may include but are not limited to: temporary seeding, permanent seeding, mulching, geotextiles, sodding, vegetative buffer strips, protection of trees, preservation of mature vegetation, and other appropriate measures. Except as provided below in II.B.1 and II.B.2, stabilization measures shall be initiated **immediately** where construction activities have temporarily or permanently ceased, but in no case more than **one (1) day** after the construction activity in that portion of the site has temporarily or permanently ceases on all disturbed portions of the site where construction will not occur for a period of fourteen (14) or more calendar days.

1. Where the initiation of stabilization measures is precluded by snow cover, stabilization measures shall be initiated as soon as practicable.
2. On areas where construction activity has temporarily ceased and will resume after fourteen (14) days, a temporary stabilization method can be used.

The following stabilization practices will be used for this project:

- |   |  |
|---|--|
| <input type="checkbox"/> Erosion Control Blanket / Mulching           | <input type="checkbox"/> Temporary Turf (Seeding, Class 7) |
| <input type="checkbox"/> Geotextiles                                  | <input checked="" type="checkbox"/> Temporary Mulching     |
| <input checked="" type="checkbox"/> Permanent Seeding                 | <input type="checkbox"/> Vegetated Buffer Strips           |
| <input type="checkbox"/> Preservation of Mature Seeding               | <input type="checkbox"/> Other (Specify) _____             |
| <input type="checkbox"/> Protection of Trees                          | <input type="checkbox"/> Other (Specify) _____             |
| <input type="checkbox"/> Sodding                                      | <input type="checkbox"/> Other (Specify) _____             |
| <input checked="" type="checkbox"/> Temporary Erosion Control Seeding | <input type="checkbox"/> Other (Specify) _____             |

Describe how the stabilization practices listed above will be utilized during construction:

The project is designed to minimize the effects of construction activities that will result in earth disturbing activities causing erosion. The phasing of the construction activities will involve only disturbing what is required and leaving the remainder of the site with established grass cover to be undisturbed. All areas exposed due to construction will utilize temporary erosion control seeding applied with mulch to minimize the potential discharge of sediment.

Describe how the stabilization practices listed above will be utilized after construction activities have been completed:

Construction activities for this project will be completed during the winter months. Subsequent construction projects will commence upon completion of this project. Areas disturbed by construction during this project will be temporarily stabilized with an application of temporary seed and mulch. Temporary stabilization will be continued or permanent stabilization placed during subsequent construction projects.

**C. Structural Practices:** Provided below is a description of structural practices that will be implemented, to the degree attainable, to divert flows from exposed soils, store flows or otherwise limit runoff and the discharge of pollutants from exposed areas of the site. Such practices may include but are not limited to: perimeter erosion barrier, earth dikes, drainage swales, sediment traps, ditch checks, subsurface drains, pipe slope drains, level spreaders, storm drain inlet protection, rock outlet protection, reinforced soil retaining systems, gabions, and temporary or permanent sediment basins. The installation of these devices may be subject to Section 404 of the Clean Water Act.

- |  |   |
|--|---|
| <input type="checkbox"/> Aggregate Ditch           | <input type="checkbox"/> Stabilized Construction Exits    |
| <input type="checkbox"/> Concrete Revetment Mats   | <input type="checkbox"/> Stabilized Trench Flow           |
| <input type="checkbox"/> Dust Suppression          | <input type="checkbox"/> Slope Mattress                   |
| <input type="checkbox"/> Dewatering Filtering      | <input type="checkbox"/> Slope Walls                      |
| <input type="checkbox"/> Gabions                   | <input checked="" type="checkbox"/> Temporary Ditch Check |
| <input type="checkbox"/> In-Stream or Wetland Work | <input type="checkbox"/> Temporary Pipe Slope Drain       |

- Level Spreaders
- Paved Ditch
- Permanent Check Dams
- Perimeter Erosion Barrier
- Permanent Sediment Basin
- Retaining Walls
- Riprap
- Rock Outlet Protection
- Sediment Trap
- Storm Drain Inlet Protection

- Temporary Sediment Basin
- Temporary Stream Crossing
- Turf Reinforcement Mats
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_
- Other (Specify) \_\_\_\_\_

Describe how the structural practices listed above will be utilized during construction:

Structural practices will be utilized to prevent sediment from being discharged off site. The perimeter barrier will be placed at locations indicated on the plans and will be installed prior to major earth disturbing activities. Temporary ditch checks will be utilized in the graded ditches.

Describe how the structural practices listed above will be utilized after construction activities have been completed:

Structural practices listed will remain in place until permanent vegetation has been established. The temporary measures will then be removed.

**D. Treatment Chemicals**

Will polymer flocculants or treatment chemicals be utilized on this project:  Yes  No

If yes above, identify where and how polymer flocculants or treatment chemicals will be utilized on this project.

**E. Permanent (i.e., Post-Construction) Storm Water Management Controls:** Provided below is a description of measures that will be installed during the construction process to control volume and pollutants in storm water discharges that will occur after construction operations have been completed. The installation of these devices may be subject to Section 404 of the Clean Water Act.

1. Such practices may include but are not limited to: storm water detention structures (including wet ponds), storm water retention structures, flow attenuation by use of open vegetated swales and natural depressions, infiltration of runoff on site, and sequential systems (which combine several practices).

The practices selected for implementation were determined based on the technical guidance in Chapter 41 (Construction Site Storm Water Pollution Control) of the IDOT BDE Manual. If practices other than those discussed in Chapter 41 are selected for implementation or if practices are applied to situations different from those covered in Chapter 41, the technical basis for such decisions will be explained below.

2. Velocity dissipation devices will be placed at discharge locations and along the length of any outfall channel as necessary to provide a non-erosive velocity flow from the structure to a water course so that the natural physical and biological characteristics and functions are maintained and protected (e.g., maintenance of hydrologic conditions such as the hydroperiod and hydrodynamics present prior to the initiation of construction activities).

Description of permanent storm water management controls:

**F. Approved State or Local Laws:** The management practices, controls and provisions contained in this plan will be in accordance with IDOT specifications, which are at least as protective as the requirements contained in the IEPA's Illinois Urban Manual. Procedures and requirements specified in applicable sediment and erosion site plans or storm water management plans approved by local officials shall be described or incorporated by reference in the space provided below. Requirements specified in sediment and erosion site plans, site permits, storm water management site plans or site permits approved by local officials that are applicable to protecting surface water resources are, upon submittal of an NOI, to be authorized to discharge under the Permit ILR10 incorporated by reference and are enforceable under this permit even if they are not specifically included in the plan.

Description of procedures and requirements specified in applicable sediment and erosion site plans or storm water management plans

approved by local officials:

N/A

**G. Contractor Required Submittals:** Prior to conducting any professional services at the site covered by this plan, the Contractor and each subcontractor responsible for compliance with the permit shall submit to the Resident Engineer a Contractor Certification Statement, BDE 2342A.

1. The Contractor shall provide a construction schedule containing an adequate level of detail to show major activities with implementation of pollution prevention BMPs, including the following items:

- Approximate duration of the project, including each stage of the project
- Rainy season, dry season, and winter shutdown dates
- Temporary stabilization measures to be employed by contract phases
- Mobilization time-frame
- Mass clearing and grubbing/roadside clearing dates
- Deployment of Erosion Control Practices
- Deployment of Sediment Control Practices (including stabilized cons
  
- Deployment of Construction Site Management Practices (including concrete washout facilities, chemical storage, refueling locations, etc.)
- Paving, saw-cutting, and any other pavement related operations
- Major planned stockpiling operation
- Time frame for other significant long-term operations or activities that may plan non-storm water discharges as dewatering, grinding, etc
- Permanent stabilization activities for each area of the project

2. During the pre-construction meeting, the Contractor and each subcontractor shall provide, as an attachment to their signed Contractor Certification Statement, a discussion of how they will comply with the requirements of the permit in regard to the following items and provide a graphical representation showing location and type of BMPs to be used when applicable:

- Temporary Ditch Checks - Identify what type and the source of Temporary Ditch Checks that will be installed as part of the project. The installation details will then be included with the SWPPP.
- Vehicle Entrances and Exits - Identify type and location of stabilized construction entrances and exits to be used and how they will be maintained.
- Material Delivery, Storage and Use - Discuss where and how materials including chemicals, concrete curing compounds, petroleum products, etc. will be stored for this project.
- Stockpile Management - Identify the location of both on-site and off-site stockpiles. Discuss what BMPs will be used to prevent pollution of storm water from stockpiles.
- Waste Disposal - Discuss methods of waste disposal that will be used for this project.
- Spill Prevention and Control - Discuss steps that will be taken in the event of a material spill (chemicals, concrete curing compounds, petroleum, etc.)
- Concrete Residuals and Washout Wastes - Discuss the location and type of concrete washout facilities to be used on this project and how they will be signed and maintained.
- Litter Management - Discuss how litter will be maintained for this project (education of employees, number of dumpsters, frequency of dumpster pick-up, etc.).
- Vehicle and Equipment Fueling - Identify equipment fueling locations for this project and what BMPs will be used to ensure containment and spill prevention.
- Vehicle and Equipment Cleaning and Maintenance - Identify where equipment cleaning and maintenance locations for this project and what BMPs will be used to ensure containment and spill prevention.
- Dewatering Activities - Identify the controls which will be used during dewatering operations to ensure sediments will not leave the construction site.
- Polymer Flocculants and Treatment Chemicals - Identify the use and dosage of treatment chemicals and provide the Resident Engineer with Material Safety Data Sheets. Describe procedures on how the chemicals will be used and identify who will be responsible for the use and application of these chemicals. The selected individual must be trained on the established procedures.
- Additional measures indicated in the plan.

### III. Maintenance:

When requested by the Contractor, the Resident Engineer will provide general maintenance guides (e.g., IDOT Erosion and Sediment Control Field Guide) to the Contractor for the practices associated with this project. Describe how all items will be checked for structural integrity, sediment accumulation and functionality. Any damage or undermining shall be repaired immediately. Provide specifics on how repairs will be made. The following additional procedures will be used to maintain, in good and effective operating conditions, the vegetation, erosion and sediment control measures and other protective measures identified in this plan. It will be the Contractor's responsibility to attain maintenance guidelines for any manufactured BMPs which are to be installed and maintained per manufacture's specifications.

#### IV. Inspections:

Qualified personnel shall inspect disturbed areas of the construction site including Borrow, Waste, and Use Areas, which have not yet been finally stabilized, structural control measures, and locations where vehicles and equipment enter and exit the site using IDOT Storm Water Pollution Prevention Plan Erosion Control Inspection Report, BC 2259. Such inspections shall be conducted at least once every seven (7) calendar days and within twenty-four (24) hours of the end of a storm or by the end of the following business or work day that is 0.5 inch or greater or equivalent snowfall.

Inspections may be reduced to once per month when construction activities have ceased due to frozen conditions. Weekly inspections will recommence when construction activities are conducted, or if there is 0.5" or greater rain event, or a discharge due to snowmelt occurs.

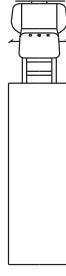
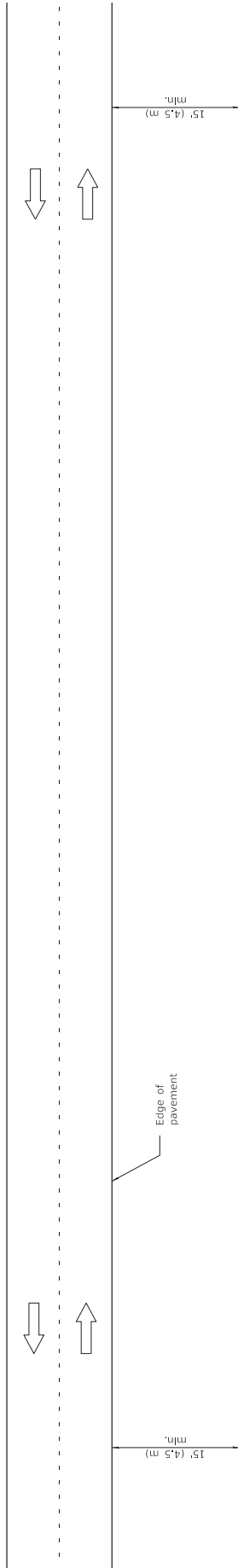
If any violation of the provisions of this plan is identified during the conduct of the construction work covered by this plan, the Resident Engineer shall notify the appropriate IEPA Field Operations Section office by email at: [epa.swnoncomp@illinois.gov](mailto:epa.swnoncomp@illinois.gov), telephone or fax within twenty-four (24) hours of the incident. The Resident Engineer shall then complete and submit an "Incidence of Non-Compliance" (ION) report for the identified violation within five (5) days of the incident. The Resident Engineer shall use forms provided by IEPA and shall include specific information on the cause of noncompliance, actions which were taken to prevent any further causes of noncompliance, and a statement detailing any environmental impact which may have resulted from the noncompliance. All reports of non-compliance shall be signed by a responsible authority in accordance with Part VI. G of the Permit ILR10.

The Incidence of Non-Compliance shall be mailed to the following address:

Illinois Environmental Protection Agency  
Division of Water Pollution Control  
Attn: Compliance Assurance Section  
1021 North Grand East  
Post Office Box 19276  
Springfield, Illinois 62794-9276

#### V. Failure to Comply:

Failure to comply with any provisions of this Storm Water Pollution Prevention Plan will result in the implementation of a National Pollutant Discharge Elimination System/Erosion and Sediment Control Deficiency Deduction against the Contractor and/or penalties under the Permit ILR10 which could be passed on to the Contractor.



**TYPICAL APPLICATIONS**

- Landscaping work
- Utility work
- Fencing contracts and maintenance
- Cleaning culverts

**GENERAL NOTES**

This Standard is used where at all times all vehicles, equipment, workers or their activities are more than 15' (4.5 m) from the edge of pavement.

When the work operation requires that two or more work vehicles cross the 15' (4.5 m) clear zone in any one hour, traffic control shall be according to Standard 701006.

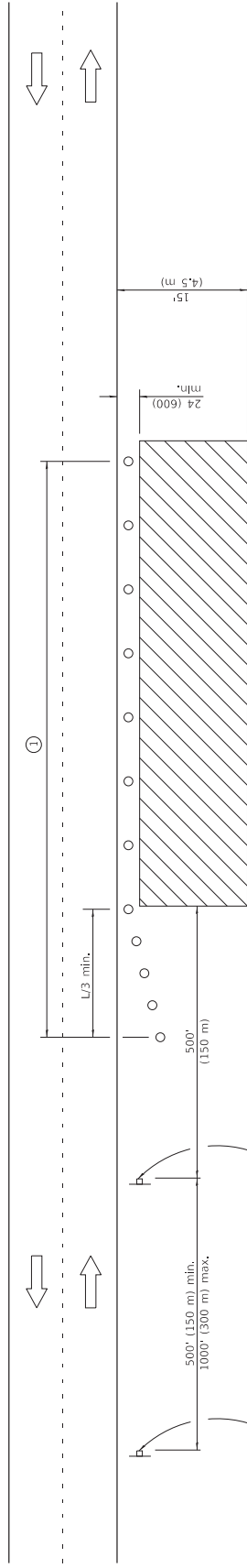
All dimensions are in inches (millimeters) unless otherwise shown.


DATE	REVISIONS
1-1-09	Switched units to English (metric).
1-1-05	Revised title and notes.


**OFF-RD OPERATIONS,  
2L, 2W, MORE THAN  
15' (4.5 m) AWAY**

**STANDARD 701001-02**

Illinois Department of Transportation PASSED ENGINEER OF OPERATIONS APPROVED ENGINEER OF DESIGN AND ENVIRONMENT	January 1, 2009 <i>[Signature]</i>	ISSUED 1-1-07
	January 1, 2009 <i>[Signature]</i>	



For contract construction projects  
 W20-1103(0)-48




For maintenance and utility projects  
 W20-110(-)-48

W21-110(-)-48

**TYPICAL APPLICATIONS**

- Utility operations
- Culvert extensions
- Side slope changes
- Guardrail installation and maintenance
- Delineator installation
- Landscaping operations
- Shoulder repair
- Sign installation and maintenance

**SYMBOLS**

-  Work area
-  Sign
-  Cone, drum or barricade

**GENERAL NOTES**

This Standard is used where any vehicles, equipment, workers or their activities will encroach in the area 15' (4.5 m) to 24 (600) from the edge of pavement.

Calculate L as follows:

SPEED LIMIT	English	FORMULAS	(Metric)
	40 mph (70 km/h) or less:	$L = \frac{WS^2}{60}$	$L = \frac{WS^2}{150}$
45 mph (80 km/h) or greater:		$L = (W)(S)$	$L = 0.65(W)(S)$

W = Width of offset in feet (meters).  
 S = Normal posted speed mph (km/h).

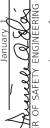

All dimensions are in inches (millimeters) unless otherwise shown.

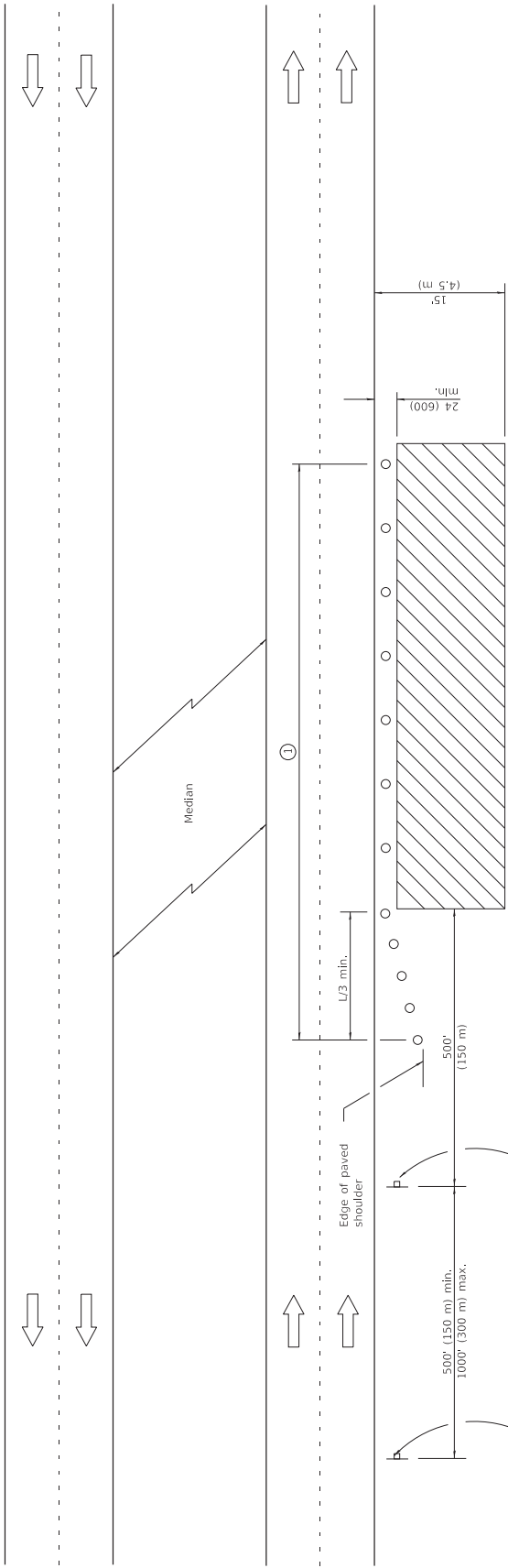
① When the work operation exceeds one hour, cones, drums or barricades shall be placed at 25' (8 m) centers for L/3 distance, and at 50' (15 m) centers through the remainder of the work area.

DATE	REVISIONS
1-1-14	Revised workers sign number to agree with current MUTCD.
1-1-13	Omitted text 'WORKERS' Sign.

**OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE**

STANDARD 701006-05

Illinois Department of Transportation  
 PASSED January 2014  
  
 ENGINEER OF SAFETY ENGINEERING  
 APPROVED January 1, 2014  
  
 ENGINEER OF DESIGN AND ENVIRONMENT



For contract construction projects

For maintenance and utility projects

W20-1103(0)-48

W20-110-48

W20-110-48

**GENERAL NOTES**

This Standard is used where any vehicles, equipment, workers or their activities will encroach in the area 15' (4.5 m) to 24' (600) from the edge of pavement.

Calculate L as follows:

SPEED LIMIT	FORMULAS
English	(Metric)
40 mph (70 km/h) or less:	$L = \frac{WS^2}{60}$ $L = \frac{WS^2}{150}$
45 mph (80 km/h) or greater:	$L = (W)(S)$ $L = 0.65(W)(S)$

W = Width of offset in feet (meters).  
 S = Normal posted speed mph (km/h).

All dimensions are in inches (millimeters) unless otherwise shown.

① When the work operation exceeds one hour, cones, drums or barricades shall be placed at 25' (8 m) centers for L/3 distance, and at 50' (15 m) centers through the remainder of the work area.

**TYPICAL APPLICATIONS**

- Utility operations
- Culvert extensions
- Side slope changes
- Guardrail installation and maintenance
- Delineator installation
- Landscaping operations
- Shoulder repair
- Sign installation and maintenance

**SYMBOLS**

- Work area
- Sign
- Cone, drum or barricade

**OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE**

DATE	REVISIONS
4-1-16	Corrected typo in title.
1-1-14	Revised workers sign number to agree with current MUTCD.

STANDARD 701101-05

Illinois Department of Transportation

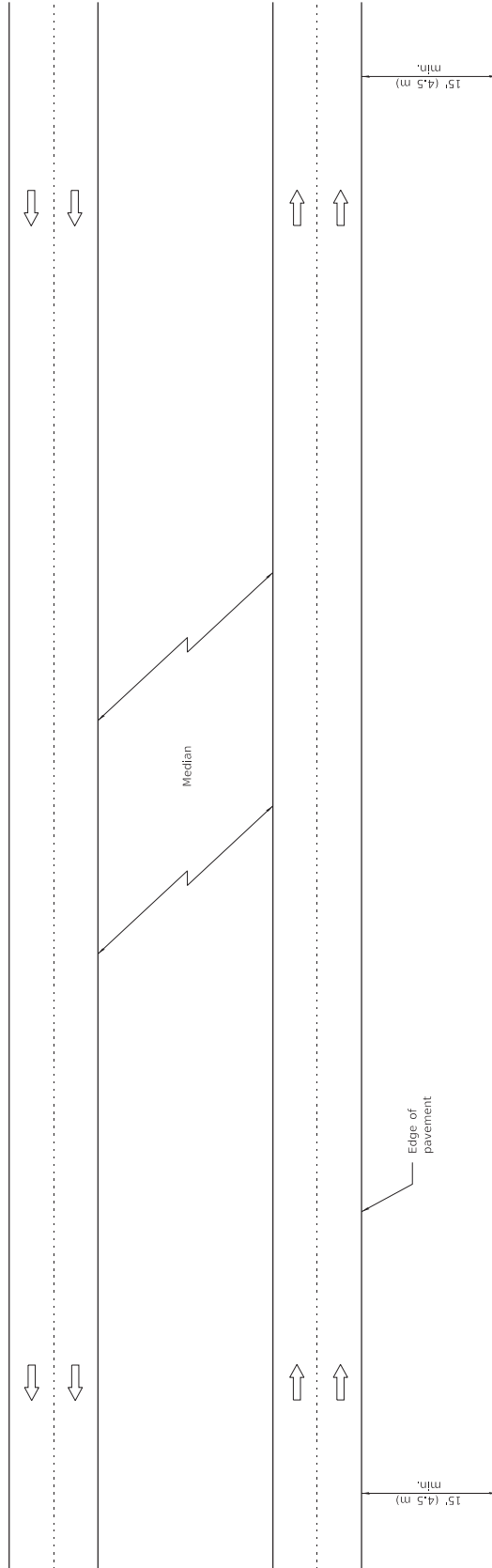
PASSED April 1, 2016

APPROVED April 1, 2016

ENGINEER OF SAFETY ENGINEERING

ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97



**GENERAL NOTES**

This Standard is used where at all times all vehicles, equipment, workers or their activities are more than 15' (4.5 m) from the edge of pavement.

When the work operation requires that two or more work vehicles cross the 15' (4.5 m) clear zone in any one hour, traffic control shall be according to Standard 701101.

This Standard also applies to work performed in the median more than 15' (4.5 m) from either pavement.

All dimensions are in inches (millimeters) unless otherwise shown.

**TYPICAL APPLICATIONS**

- Landscaping work
- Utility work
- Fencing contracts

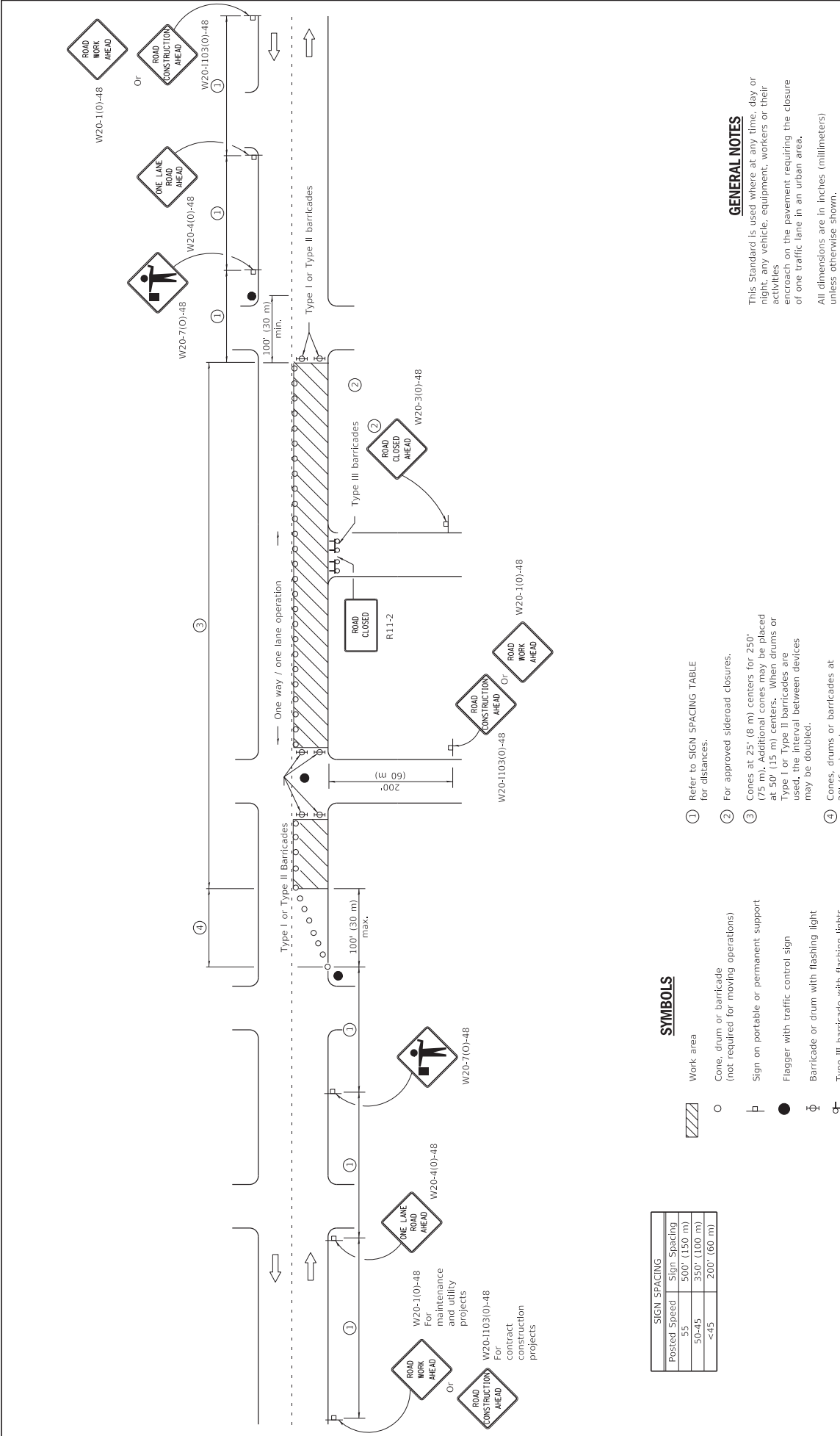
**OFF-RD OPERATIONS, MULTILANE,  
MORE THAN 15' (4.5 m) AWAY**

STANDARD 701106-02

DATE	REVISIONS
1-1-05	Switched units to English (metric).
1-1-05	Revised title.

	PASSED January 1, 2009 <i>[Signature]</i> ENGINEER OF OPERATIONS	ISSUED 1-1-07
	APPROVED January 1, 2009 <i>[Signature]</i> ENGINEER OF DESIGN AND ENVIRONMENT	





SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

**SYMBOLS**

- Work area
- Cone, drum or barricade (not required for moving operations)
- Sign on portable or permanent support
- Flagger with traffic control sign
- Barricade or drum with flashing light
- Type III barricade with flashing lights

- ① Refer to SIGN SPACING TABLE for distances.
- ② For approved sideroad closures.
- ③ Cones at 25' (8 m) centers for 250' (75 m). Additional cones may be placed at 50' (15 m) centers. When drums or Type I or Type II barricades are used, the interval between devices may be doubled.
- ④ Cones, drums or barricades at 20' (6 m) centers.

**GENERAL NOTES**

This Standard is used where at any time, day or night, any vehicle, equipment, workers or their activities encroach on the pavement requiring the closure of one traffic lane in an urban area.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-11	Revised flagger sign.
1-1-09	Switched units to English (metric).
	Corrected sign No. 5.

**URBAN LANE CLOSURE,  
2L, 2W, UNDIVIDED**

**STANDARD 701501-06**

Illinois Department of Transportation

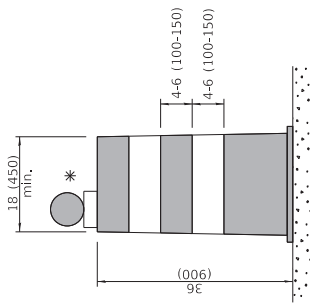
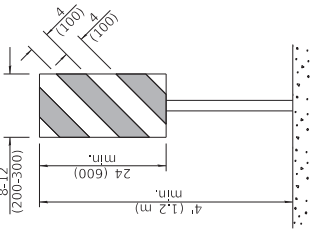
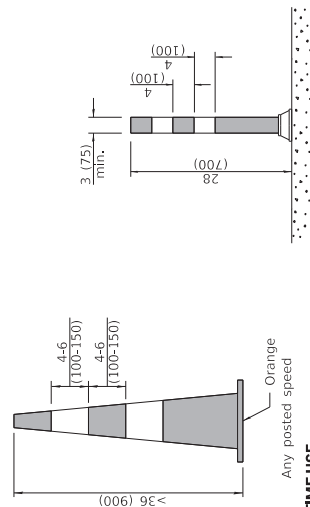
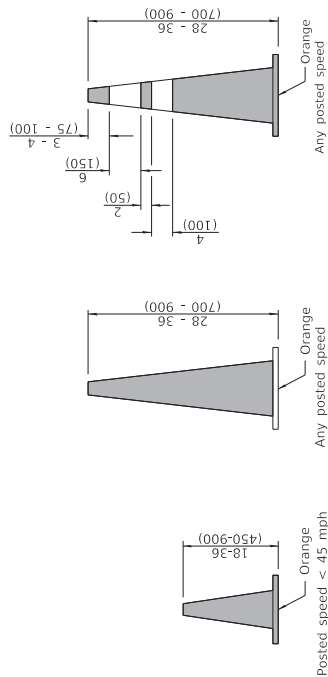
PASSED January 1, 2011

APPROVED January 1, 2011

ENGINEER OF SAFETY ENGINEERING

ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-07



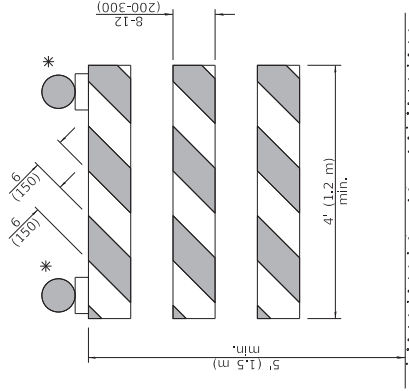
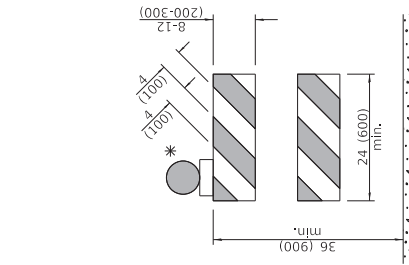
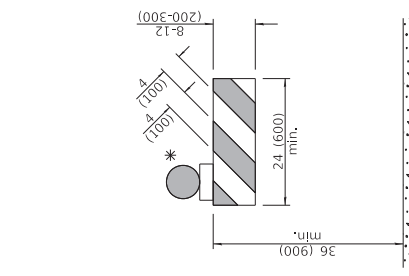
**CONES**

**DAY OR NIGHTTIME USE**

**TUBULAR MARKER**

**VERTICAL PANEL  
POST MOUNTED**

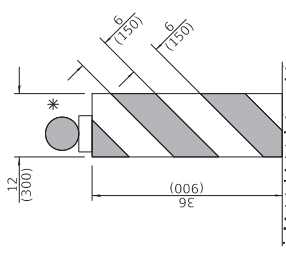
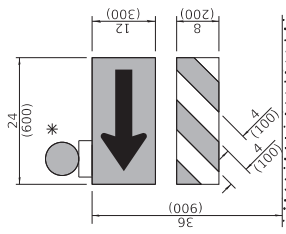
**DRUM**



**TYPE I BARRICADE**

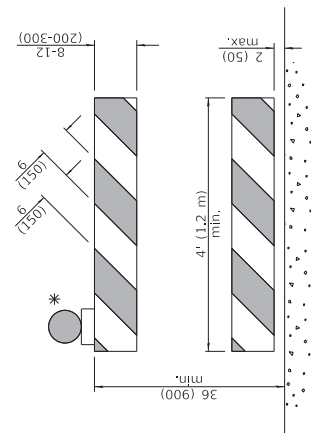
**TYPE II BARRICADE**

**TYPE III BARRICADE**



**DIRECTION INDICATOR  
BARRICADE**

**VERTICAL BARRICADE**



\* Warning lights (if required)

**GENERAL NOTES**

All heights shown shall be measured above the pavement surface.  
All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Revised cone usage and added cones >36" (900 m) height.
1-1-18	Revised END WORK ZONE SPEED LIMIT sign from orange to white background.

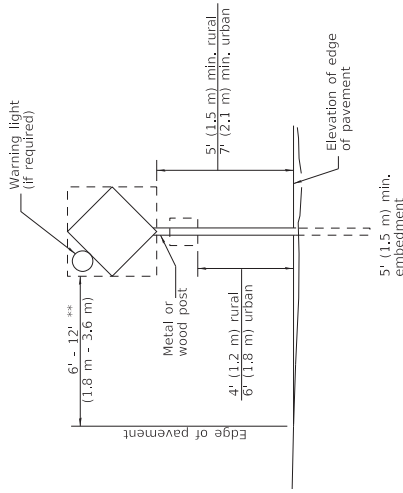
**TRAFFIC CONTROL  
DEVICES**

(Sheet 1 of 3)

**STANDARD 701901-08**

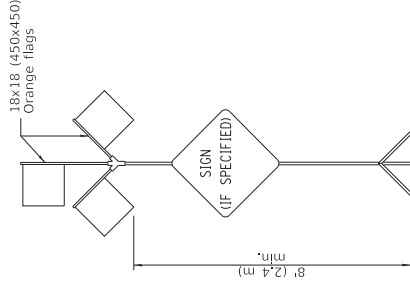
Illinois Department of Transportation  
APPROVED January 1, 2019  
ENGINEER OF SAFETY PROC. AND ENGINEERING  
APPROVED January 1, 2019  
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-13



**POST MOUNTED SIGNS**

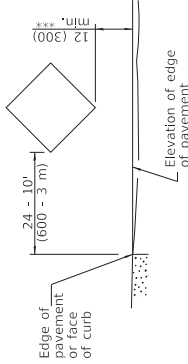
\*\* When curb or paved shoulder are present this dimension shall be 24 (600) to the face of curb or 6' (1.8 m) to the outside edge of the paved shoulder.



**HIGH LEVEL WARNING DEVICE**

**SIGNS ON TEMPORARY SUPPORTS**

\*\*\* When work operations exceed four days, this dimension shall be 5' (1.5 m) to the face of curb behind other devices; the height shall be sufficient to be seen completely above the devices.



ROAD CONSTRUCTION NEXT X MILES  
G20-1104(0)-6036

END CONSTRUCTION  
G20-1105(0)-6024

This signing is required for all projects 2 miles (3200 m) or more in length.  
ROAD CONSTRUCTION NEXT X MILES sign shall be placed 500' (150 m) in advance of project limits.  
END CONSTRUCTION sign shall be erected at the end of the job unless another job is within 2 miles (3200 m).  
Dual sign displays shall be utilized on multi-lane highways.

**WORK LIMIT SIGNING**



W21-1115(0)-3618  
R2-1-3648  
R10-1108p-3618 \*\*\*\*  
R2-1106p-3618

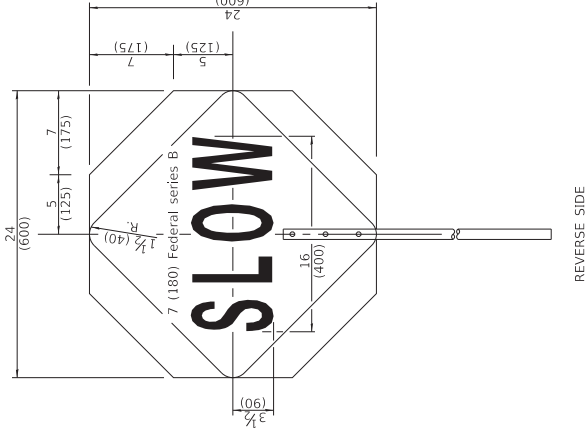
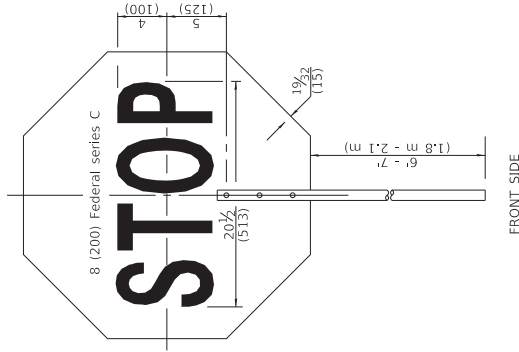
Sign assembly as shown on Standards or as allowed by District Operations.



This sign shall be used when the above sign assembly is used.

**HIGHWAY CONSTRUCTION SPEED ZONE SIGNS**

\*\*\*\* R10-1108p shall only be used along roadways under the jurisdiction of the State.



W12-1103-4848

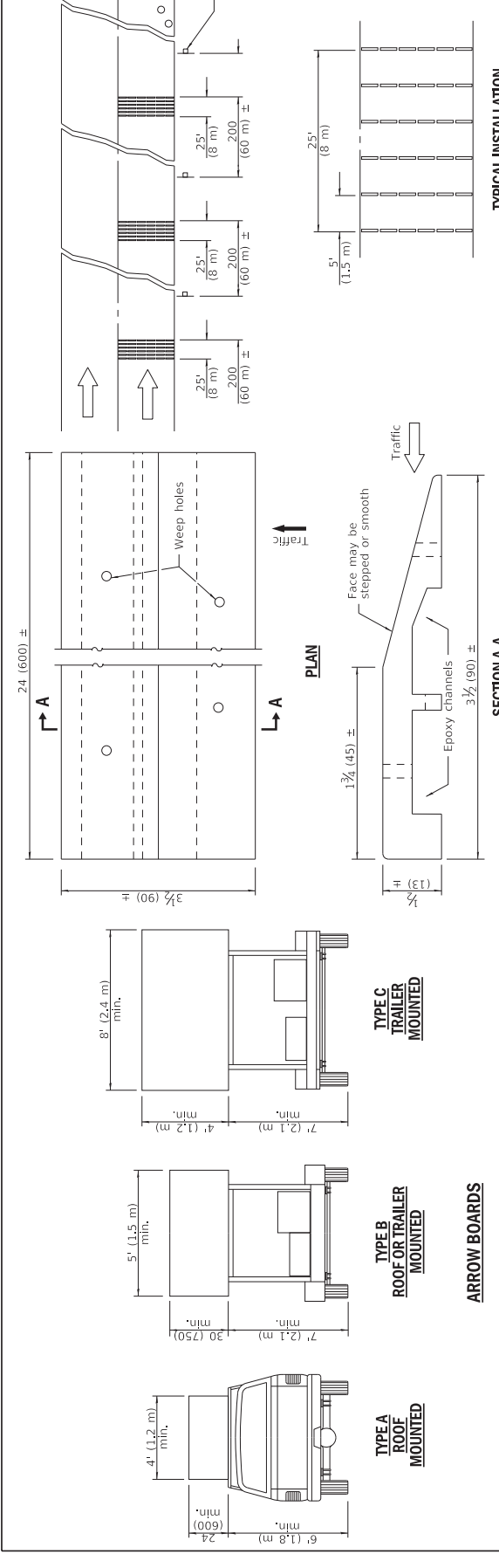
**WIDTH RESTRICTION SIGN**

XX-XX" width and X miles are variable.

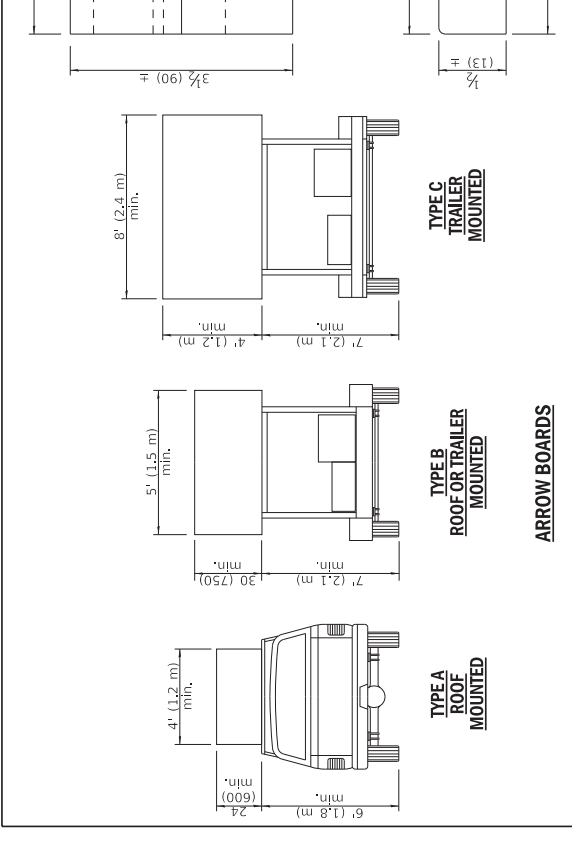
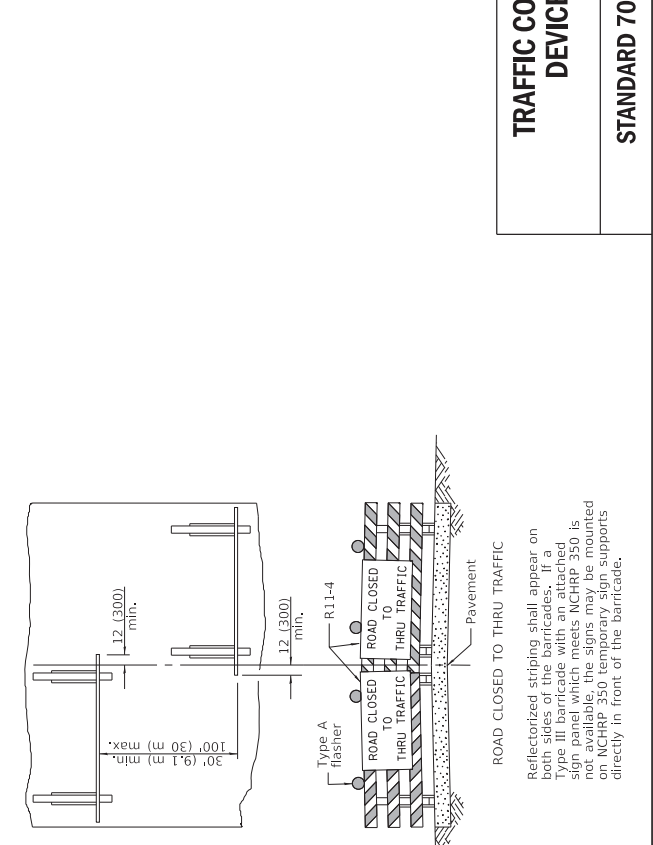
Illinois Department of Transportation  
APPROVED January 1, 2019  
Cynthia C. [Signature]  
ENGINEER OF SAFETY PROC. AND ENGINEERING  
APPROVED January 1, 2019  
S. [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-13

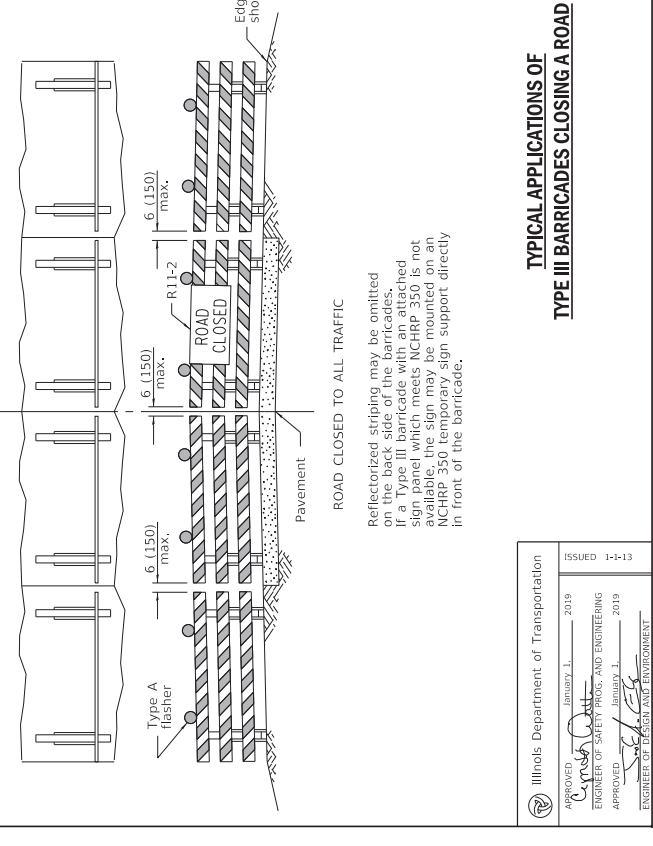
**TRAFFIC CONTROL DEVICES**  
STANDARD 701901-08  
(Sheet 2 of 3)



**TEMPORARY RUMBLE STRIPS**



**TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD**



ReflectORIZED striping shall appear on both sides of the barricades. If a Type III barricade will be used, the sign panels which MCHRP 350 is not available, the signs may be mounted directly in front of the barricade.

ReflectORIZED striping shall appear on both sides of the barricades. If a Type III barricade will be used, the sign panels which MCHRP 350 is not available, the signs may be mounted directly in front of the barricade.

Illinois Department of Transportation

APPROVED January 1, 2019

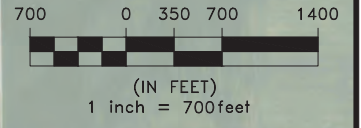
APPROVED January 1, 2019

APPROVED January 1, 2019

ISSUED 1-1-13

ENGINEER OF DESIGN AND ENVIRONMENT





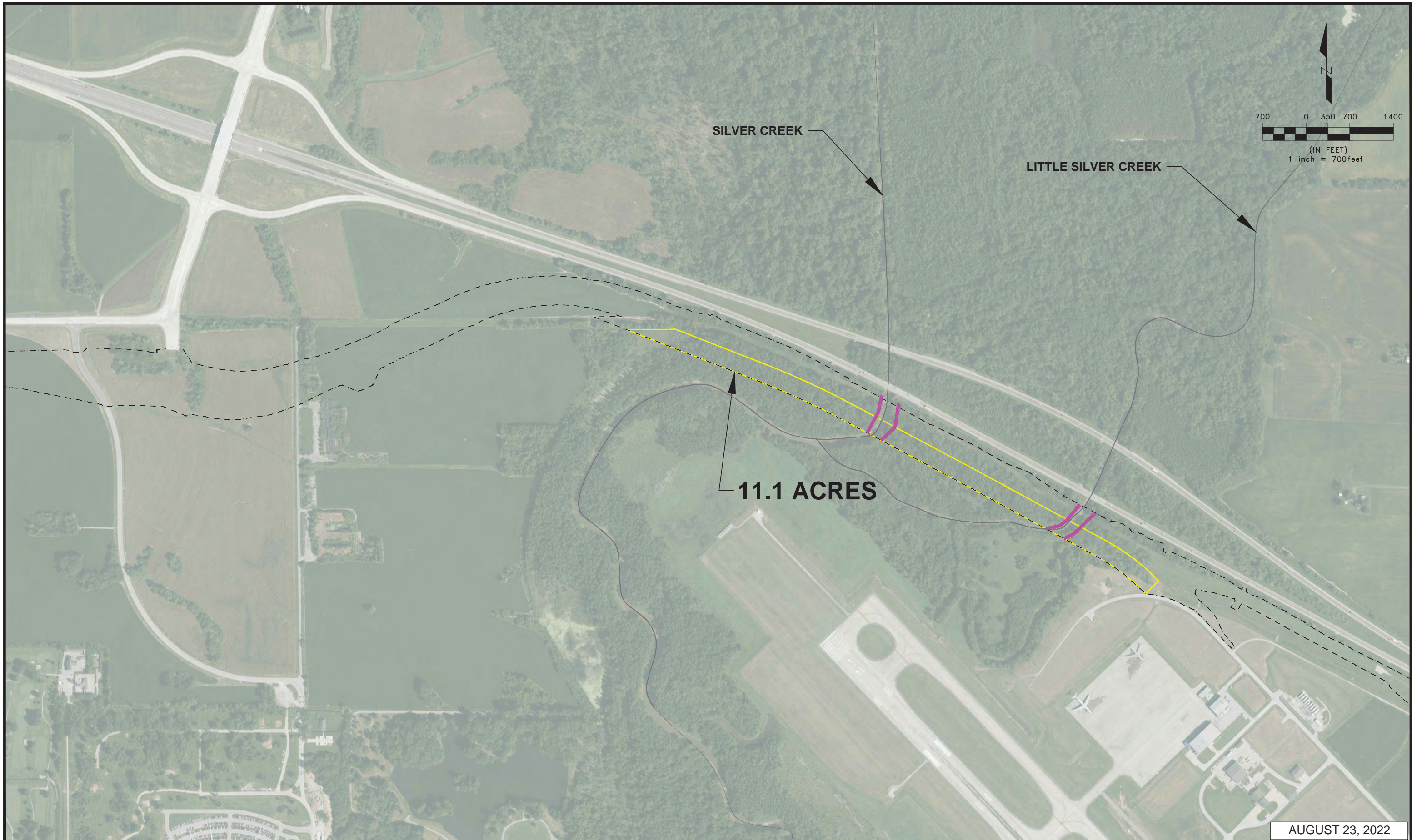
AUGUST 23, 2022

- TREE CLEARING AREA 1
- TREE CLEARING AREA 2
- TREE CLEARING AREA 3
- TREE CLEARING AREA 4
- TREE CLEARING AREA 5
- PROJECT LIMITS
- PERIMETER EROSION BARRIER



## MIDAMERICA METROLINK EXTENSION TREE CLEARING AREAS





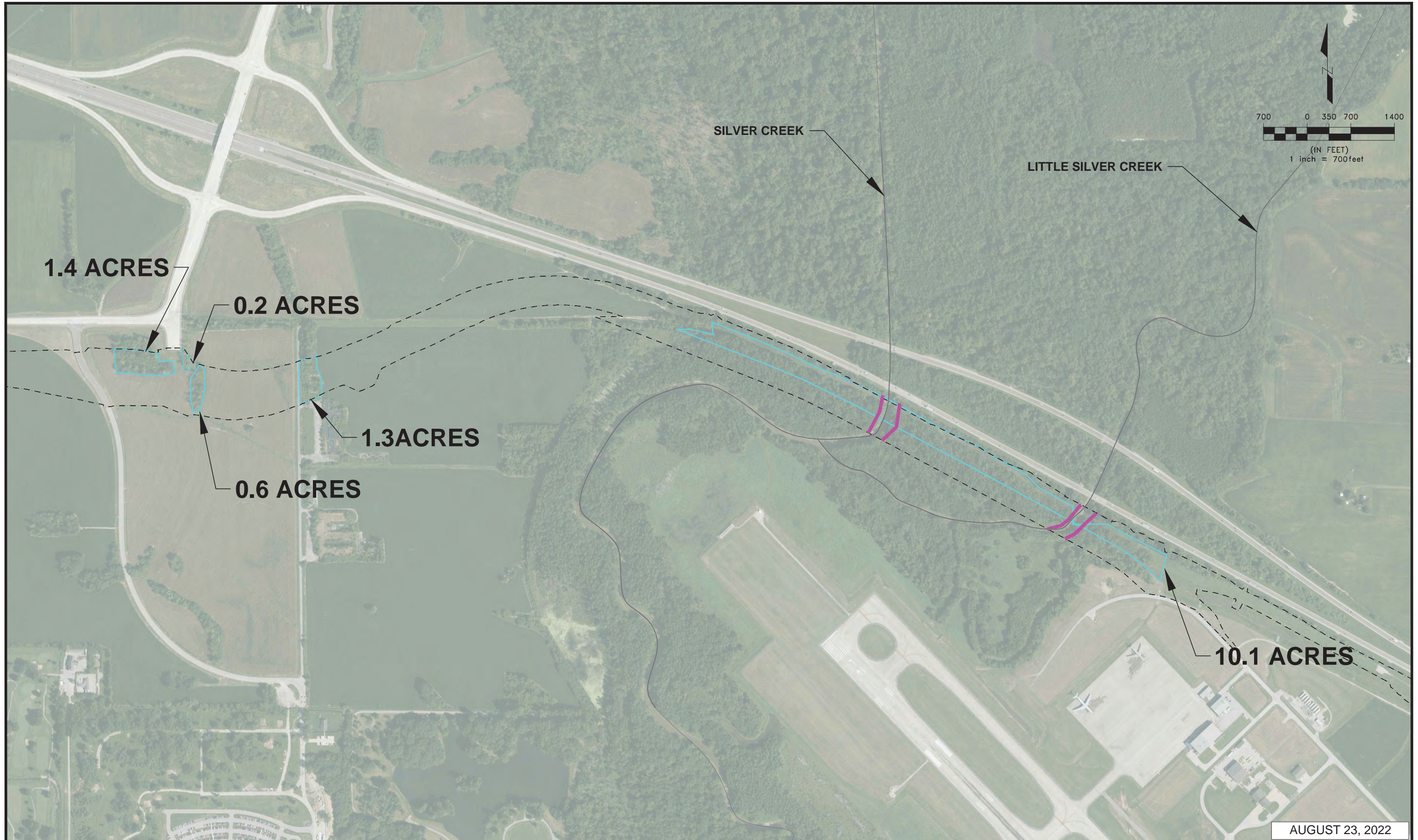
AUGUST 23, 2022

	TREE CLEARING AREA 1		TREE CLEARING AREA 5
	TREE CLEARING AREA 2		PROJECT LIMITS
	TREE CLEARING AREA 3		PERIMETER EROSION BARRIER
	TREE CLEARING AREA 4		



MIDAMERICA METROLINK EXTENSION  
TREE CLEARING AREA 1





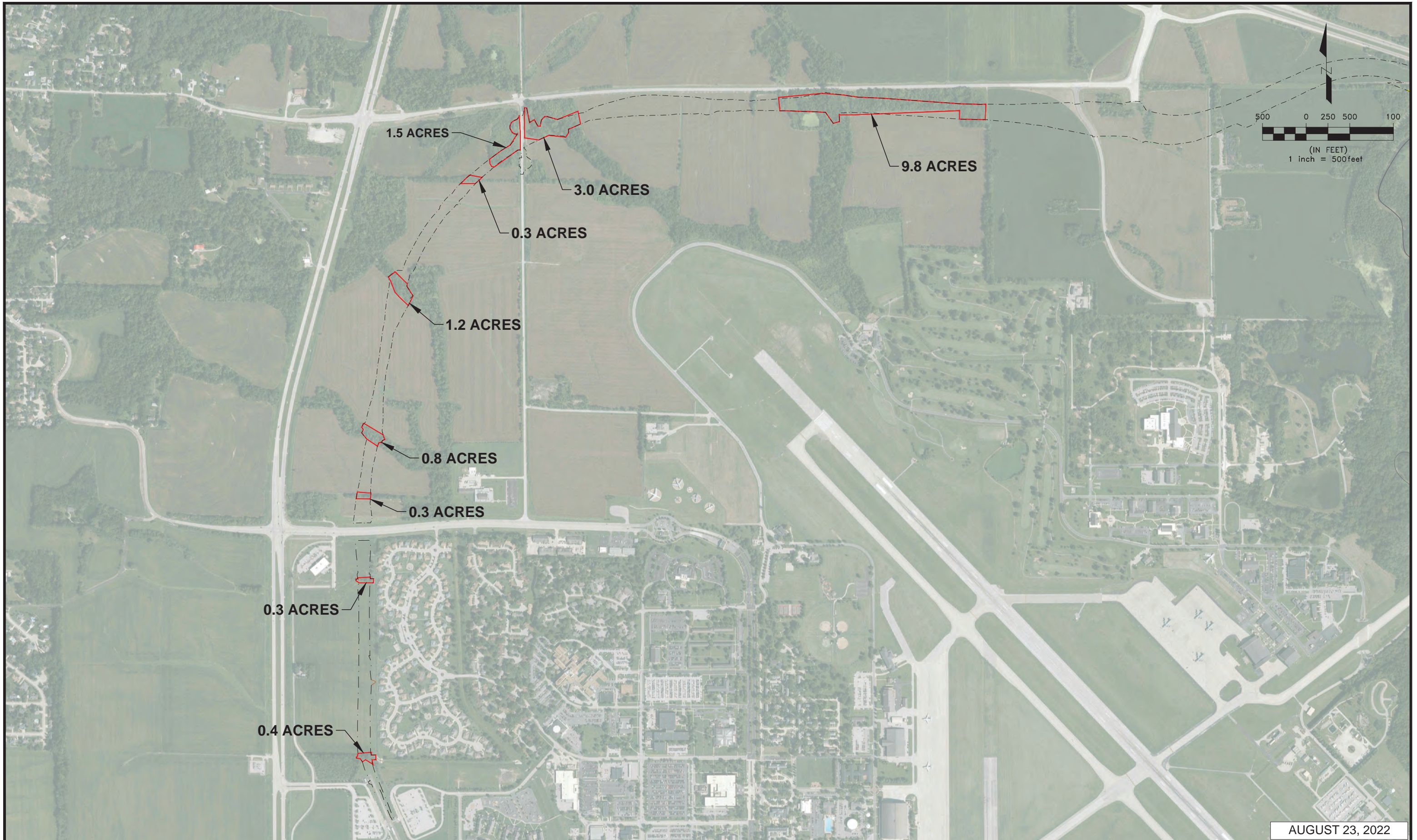
AUGUST 23, 2022

- |  |                      |   |                           |
|--|----------------------|---|---------------------------|
|  | TREE CLEARING AREA 1 |  | TREE CLEARING AREA 5      |
|  | TREE CLEARING AREA 2 |  | PROJECT LIMITS            |
|  | TREE CLEARING AREA 3 |  | PERIMETER EROSION BARRIER |
|  | TREE CLEARING AREA 4 |   |                           |










**MIDAMERICA METROLINK EXTENSION  
TREE CLEARING AREA 2**





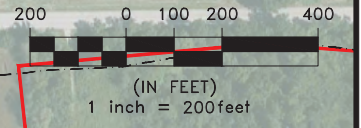
AUGUST 23, 2022

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	TREE CLEARING AREA 2		PROJECT LIMITS
	TREE CLEARING AREA 3		PERIMETER EROSION BARRIER
	TREE CLEARING AREA 4		



**MIDAMERICA METROLINK EXTENSION  
TREE CLEARING AREA 3 (1 of 2)**





**1.5 ACRES**

**3.0 ACRES**

**0.3 ACRES**

**1.2 ACRES**

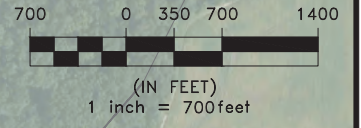
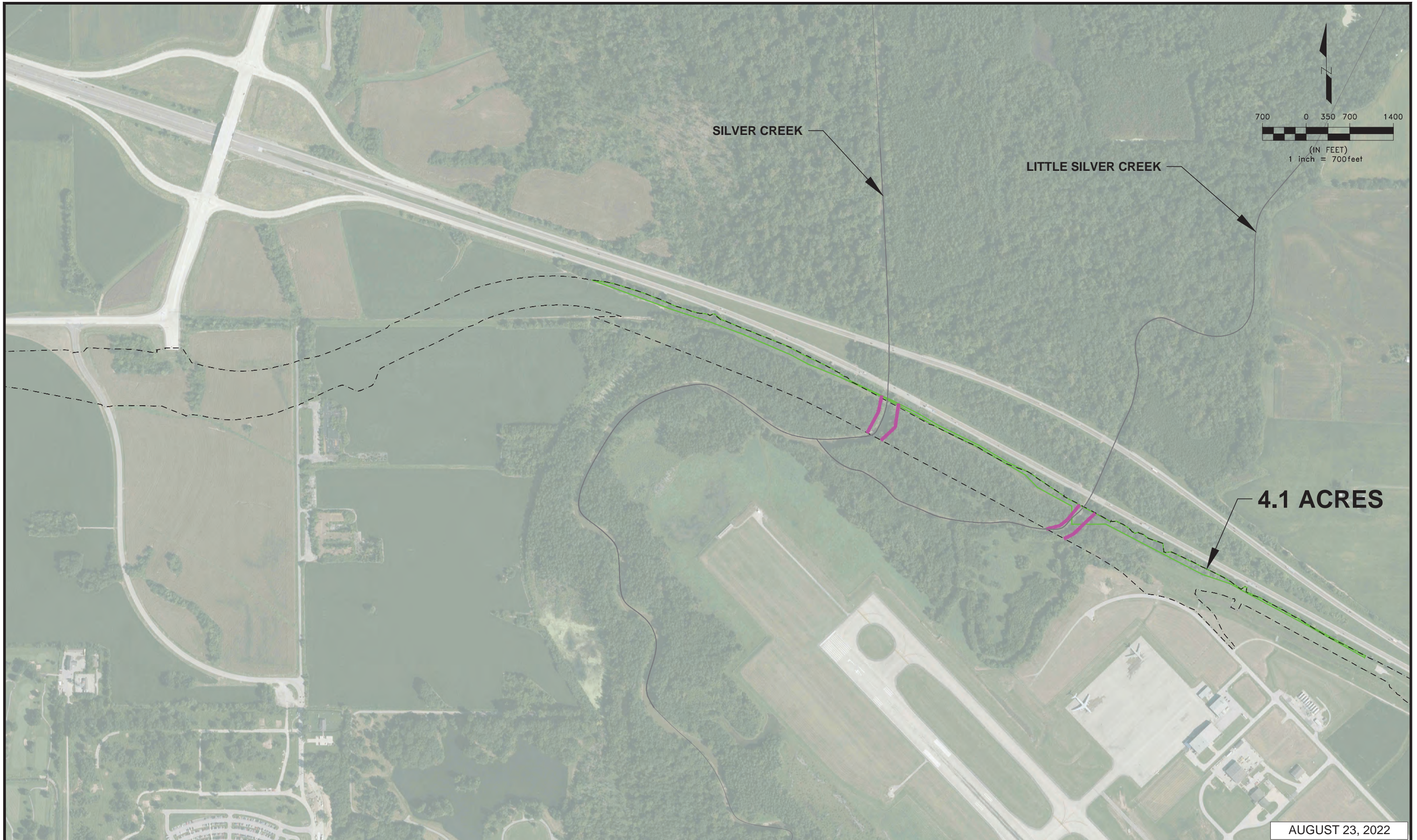
- TREE CLEARING AREA 1
- TREE CLEARING AREA 2
- TREE CLEARING AREA 3
- TREE CLEARING AREA 4
- TREE CLEARING AREA 5
- PROJECT LIMITS
- PERIMETER EROSION BARRIER



**MIDAMERICA METROLINK EXTENSION  
TREE CLEARING AREA 3 (2 of 2)**

AUGUST 23, 2022





SILVER CREEK

LITTLE SILVER CREEK

4.1 ACRES

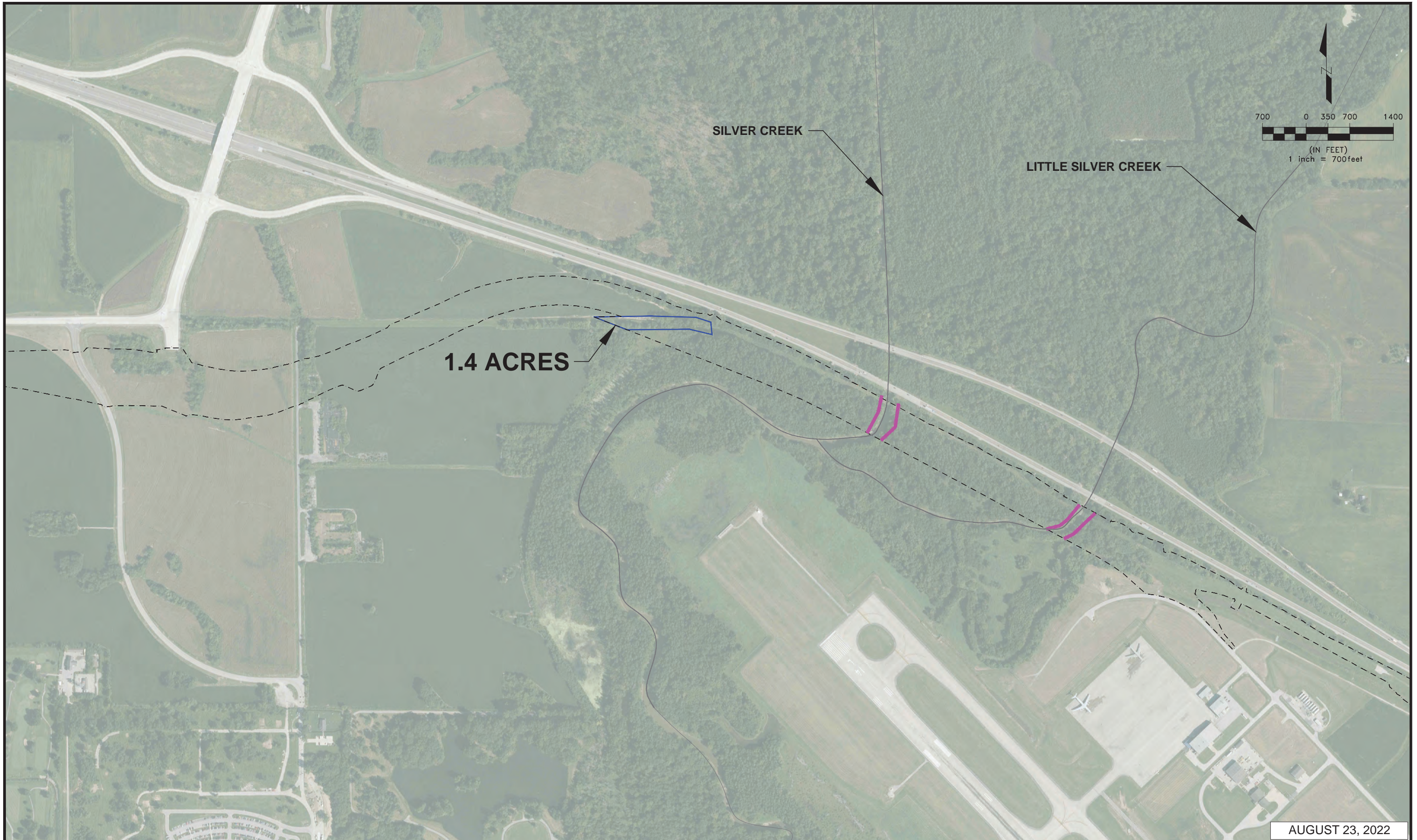
AUGUST 23, 2022

- TREE CLEARING AREA 1
- TREE CLEARING AREA 2
- TREE CLEARING AREA 3
- TREE CLEARING AREA 4
- TREE CLEARING AREA 5
- PROJECT LIMITS
- PERIMETER EROSION BARRIER

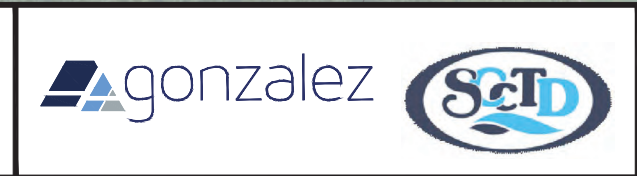


**MIDAMERICA METROLINK EXTENSION  
TREE CLEARING AREA 4**





	TREE CLEARING AREA 1		TREE CLEARING AREA 5
	TREE CLEARING AREA 2		PROJECT LIMITS
	TREE CLEARING AREA 3		PERIMETER EROSION BARRIER
	TREE CLEARING AREA 4		



MIDAMERICA METROLINK EXTENSION  
 TREE CLEARING AREA 5



## GENERAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (STANDARD SPECIFICATIONS), 2022 EDITION, UNLESS APPROVED BY THE ENGINEER.
2. ALL TREE REMOVAL NECESSARY SHALL BE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS.
3. PR. AGGREGATE SURFACE COURSE, TYPE A SHALL BE CA-6 AND BE IN ACCORDANCE WITH SECTION 402 OF THE STANDARD SPECIFICATIONS.
4. PR. SUBBASE GRANULAR MATERIAL, TYPE B SHALL BE CA-2 AND BE IN ACCORDANCE WITH SECTION 311 OF THE STANDARD SPECIFICATIONS.
5. PR. EARTHWORK SHALL BE IN ACCORDANCE WITH SECTIONS 202, 204 AND 205 OF THE STANDARD SPECIFICATIONS.
6. AT A MINIMUM, REMOVE ANY ORGANIC MATERIALS, SOFT SOIL, OR UNCONTROLLED FILL PRIOR TO THE PLACEMENT OF EMBANKMENT AND/OR SUBBASE GRANULAR MATERIAL, PER THE DIRECTION OF THE ENGINEER.

## EROSION CONTROL

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 280001 AND APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS.
2. ALL EMBANKMENT SHALL BE SEEDED WITH TEMPORARY EROSION CONTROL SEEDING, IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS.
3. ALL RIPRAP SHALL BE CLASS A5 IN ACCORDANCE WITH SECTION 281 OF THE STANDARD SPECIFICATIONS. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SECTION 282 OF THE STANDARD SPECIFICATIONS.

## TRAFFIC CONTROL

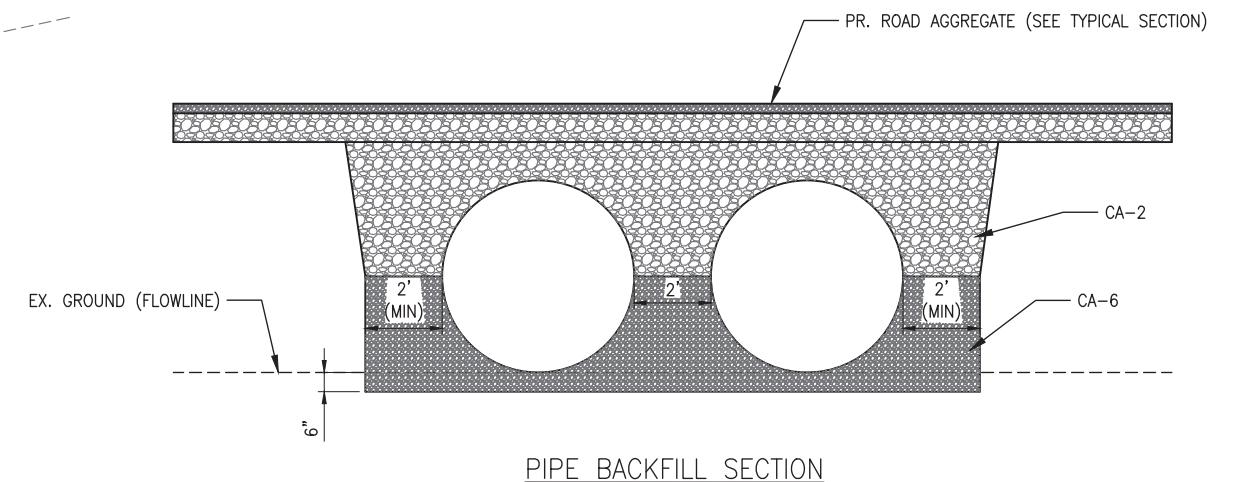
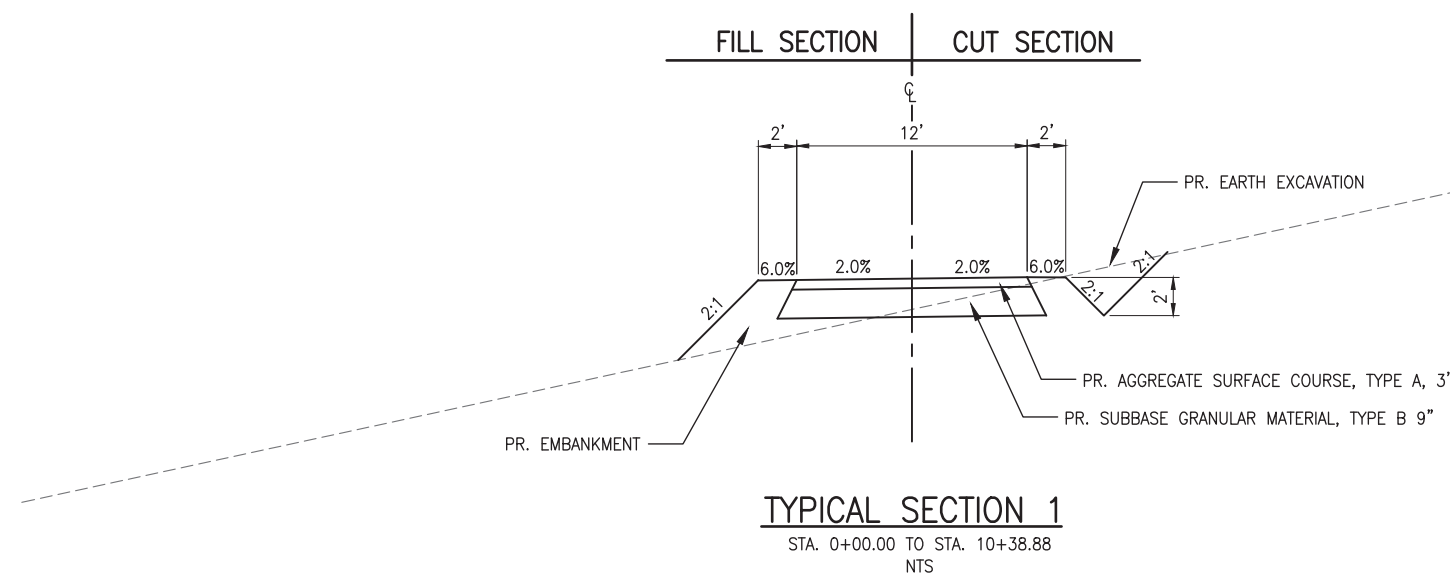
1. AIRPORT BOULEVARD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. OVERNIGHT LANE CLOSURES WILL NOT BE ALLOWED.
2. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701501. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN IN PLACE THROUGHOUT THE DURATION OF THE PROJECT.

## LOW FLOW CROSSINGS

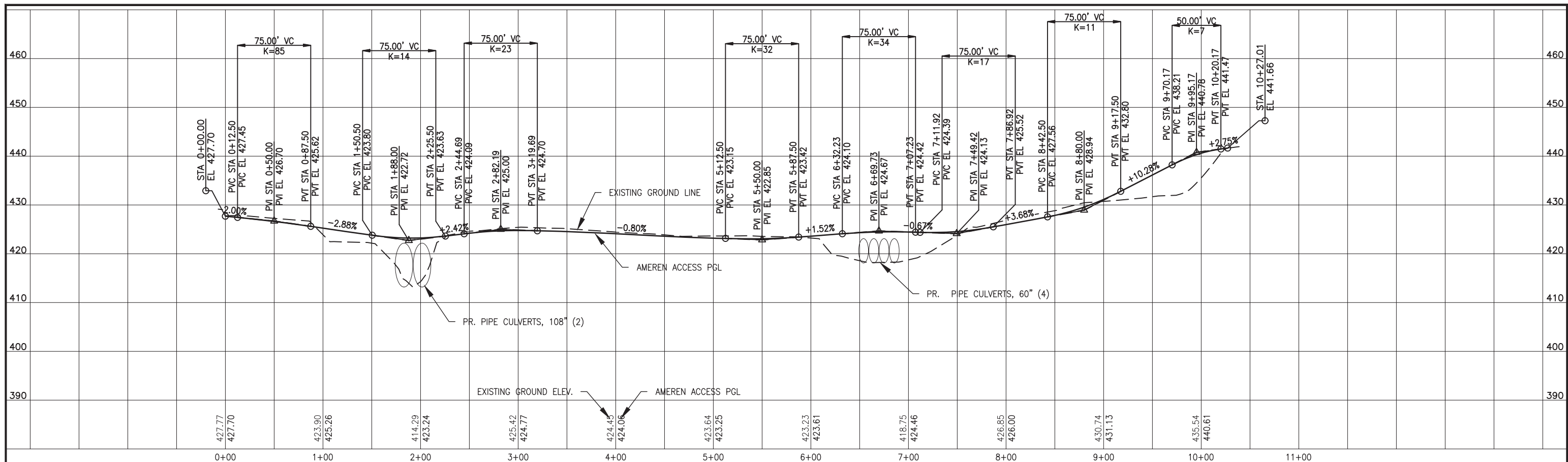
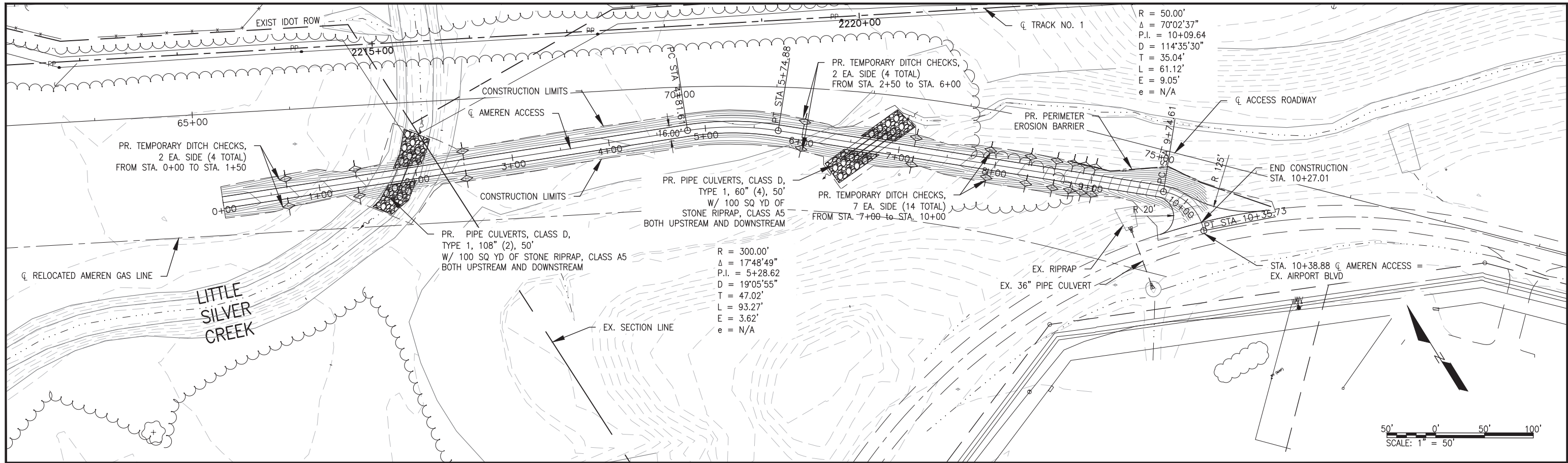
1. ALL PIPE CULVERTS, CLASS D, TYPE 1 SHALL BE IN ACCORDANCE WITH SECTION 542 OF THE STANDARD SPECIFICATIONS. THE MATERIAL SHALL BE GALVANIZED CORRUGATED STEEL OR CORRUGATED ALUMINUM IN ACCORDANCE WITH ARTICLE 542.03.
2. PIPE CULVERTS SHALL BE CONSTRUCTED ACCORDING TO ARTICLE 542.06 (METHOD III CONSTRUCTION). PIPE CULVERTS SHALL BE BACKFILLED WITH CA-2 COARSE AGGREGATE AND COMPACTED TO THE SATISFACTION OF THE ENGINEER. THE LIMITS OF THE BACKFILL SHALL EXTEND TO, AT MINIMUM, 2' OUTSIDE THE OUTER WALL OF THE PIPE CULVERT (SEE PIPE BACKFILL SECTION).
3. THE PLANS HAVE FLOWLINE ELEVATIONS ASSIGNED TO EACH CULVERT. THE ELEVATIONS CAN BE ADJUSTED IN THE FIELD TO MATCH THE EXISTING FLOW LINE, PER THE DIRECTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL MONITOR THE INLETS AND OUTLETS OF THE CULVERTS FOR ANY SCOUR THAT MAY OCCUR. IF SCOUR IS OBSERVED, THE CONTRACTOR SHALL RESTORE THE FLOWLINE TO THE ORIGINAL CHANNEL GEOMETRY.
5. THE CONTRACTOR SHALL MONITOR THE WEATHER AND WHEN HIGH FLOWS ARE EXPECTED, THEY SHALL CEASE OPERATIONS AND REMOVE ALL EQUIPMENT FROM THE WORK AREA.
6. IF HIGH FLOWS ARE EXPERIENCED, RE-TOPPING OF THE ACCESS ROAD MAY BE REQUIRED, PER THE DIRECTION OF THE ENGINEER.

## IDOT HIGHWAY STANDARDS

280001-07	TEMPORARY EROSION CONTROL SYSTEMS
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-08	TRAFFIC CONTROL DEVICES



	DESIGNED: JWS		DISCLAIMER OF RESPONSIBILITY I hereby specify that the documents intended to be authenticated by my seal are limited to this sheet, and I hereby disclaim any responsibility for all other Drawings, Specifications, Estimates, Reports or other documents or instruments relating to or intended to be used for any part of the architectural or engineering project or survey.	 Gonzalez Companies, LLC 525 W. Main St., Ste. 125 Belleville, IL 62220	 St. Clair County Transit District	 ST. CLAIR COUNTY METROLINK EXTENSION SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT	<b>AMEREN WATER X-ING TYPICAL SECTIONS</b>	AMEREN WATER CROSSING CONTRACT NO. DRAWING NO. TxCKTS01 SHEET NO.	
	DRAWN: DCL		WSP USA 211 N. Broadway, Ste. 2800 St. Louis, MO. 63102		 TWM, Inc. 4940 Old Collinsville Road Swansea, IL. 62226				
	CHECKED: KMO		DATE: 08-04-22						
	APPROVED: SJK		DATE: 08-04-22						
REV	DATE	BY	APP.	DESCRIPTION					



REV	DATE	BY	APP.	DESCRIPTION

DESIGNED:	JWS
DRAWN:	DCL
CHECKED:	KMO
APPROVED:	SJK
DATE:	08-04-22

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62775 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

EXPIRES: 11-30-2023

**gonzalez** Gonzalez Companies, LLC  
 525 W. Main St., Ste. 125  
 Belleville, IL 62220

**wsp** WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

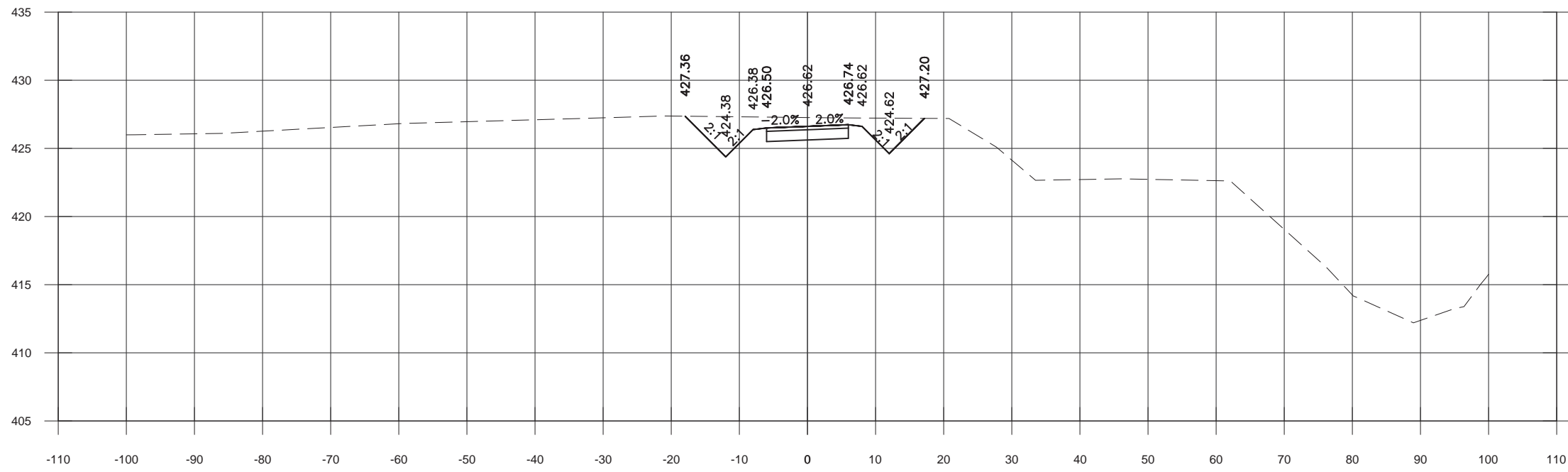
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



ST. CLAIR COUNTY METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

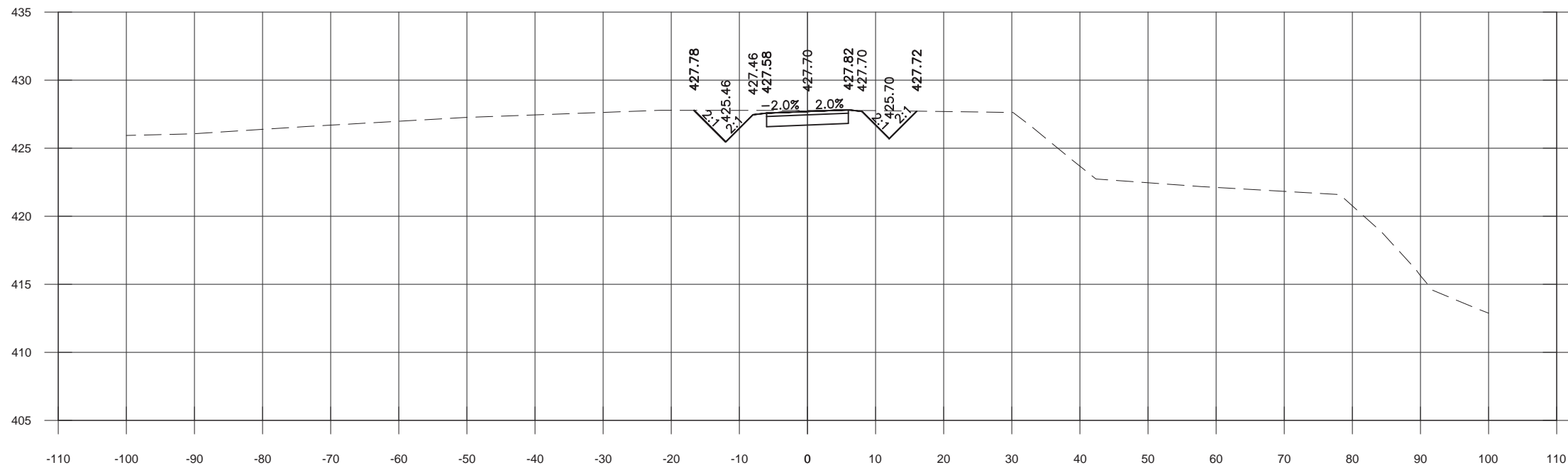
**AMEREN WATER X-ING  
 PLAN AND PROFILE  
 STA. 0+00 TO  
 STA. 10+38.88**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxCKPP01
SHEET NO.



EARTHWORK STA. 0+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	52.28	78.53	78.53
FILL	0.00	0.02	0.02

0+50



EARTHWORK STA. 0+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	32.53	0.00	0.00
FILL	0.02	0.00	0.00

0+00

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED: DCL  
 DRAWN: DCL  
 CHECKED: JWS  
 APPROVED: SJK  
 DATE: 08-04-22



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 525 W. Main St., Ste. 125  
 Belleville, IL 62220

**wsp** WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

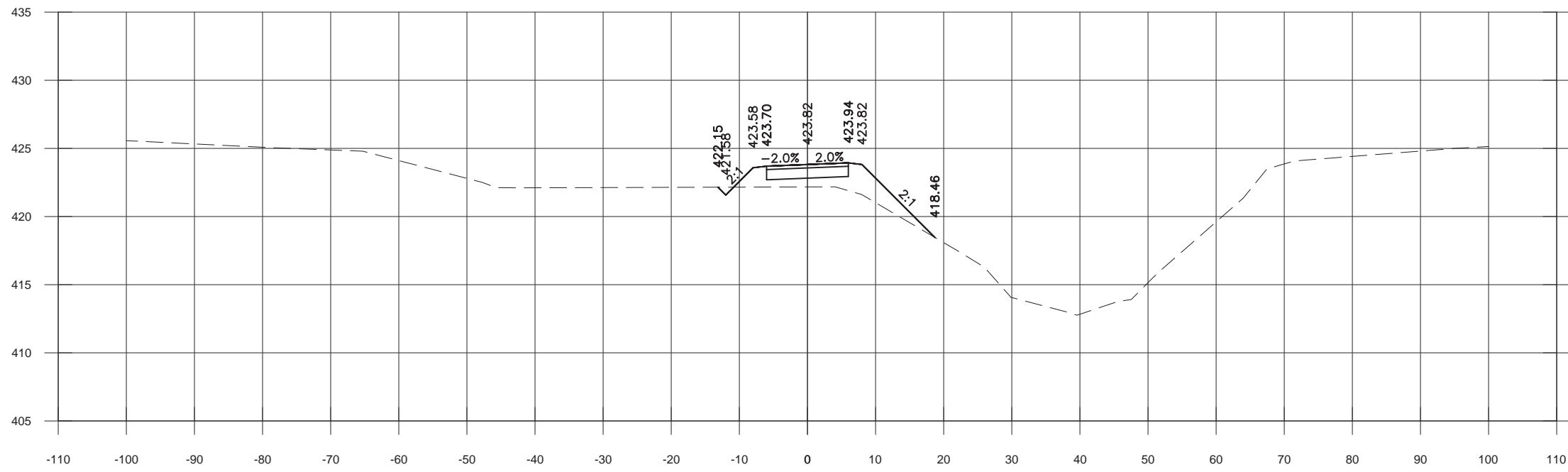
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

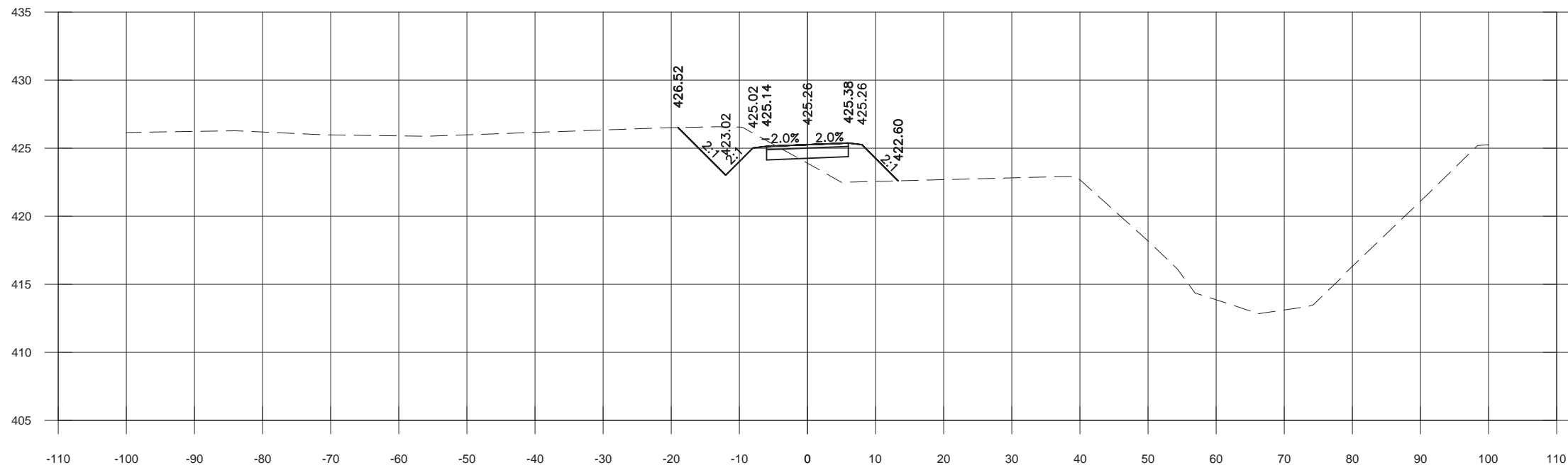
**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 0+00 TO  
 STA. 0+50**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS001  
 SHEET NO.



EARTHWORK STA. 1+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	0.67	25.79	177.91
FILL	29.07	52.74	74.54

1+50



EARTHWORK STA. 1+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	27.19	73.59	152.11
FILL	20.46	21.79	21.81

1+00

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED: DCL  
 DRAWN: DCL  
 CHECKED: JWS  
 APPROVED: SJK  
 DATE: 08-04-22

DATE: 08-04-22

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 525 W. Main St., Ste. 125  
 Belleville, IL 62220

WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226

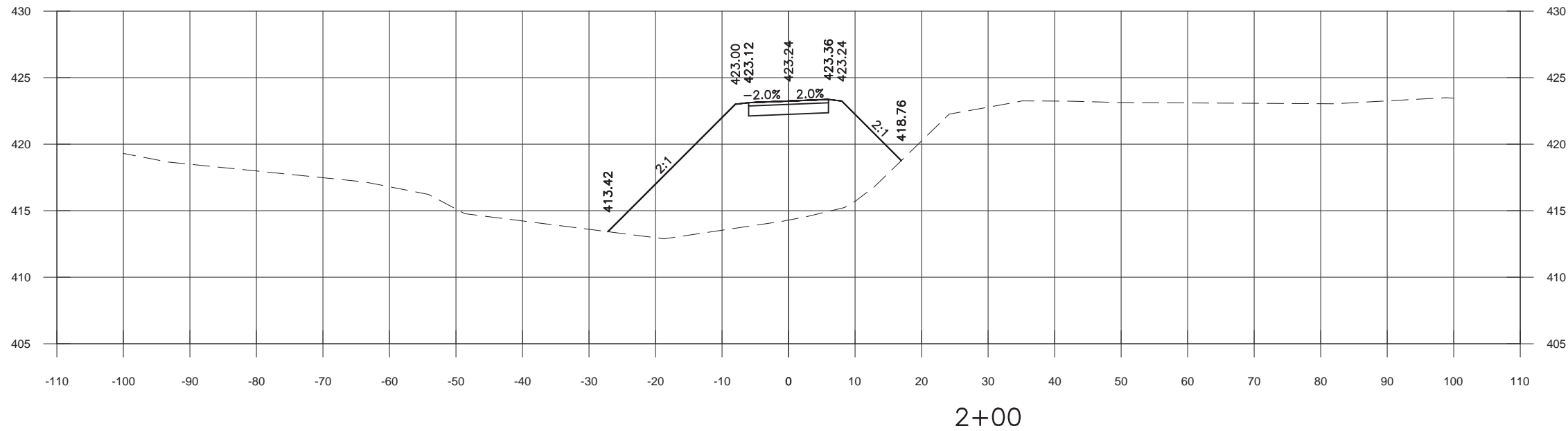


ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

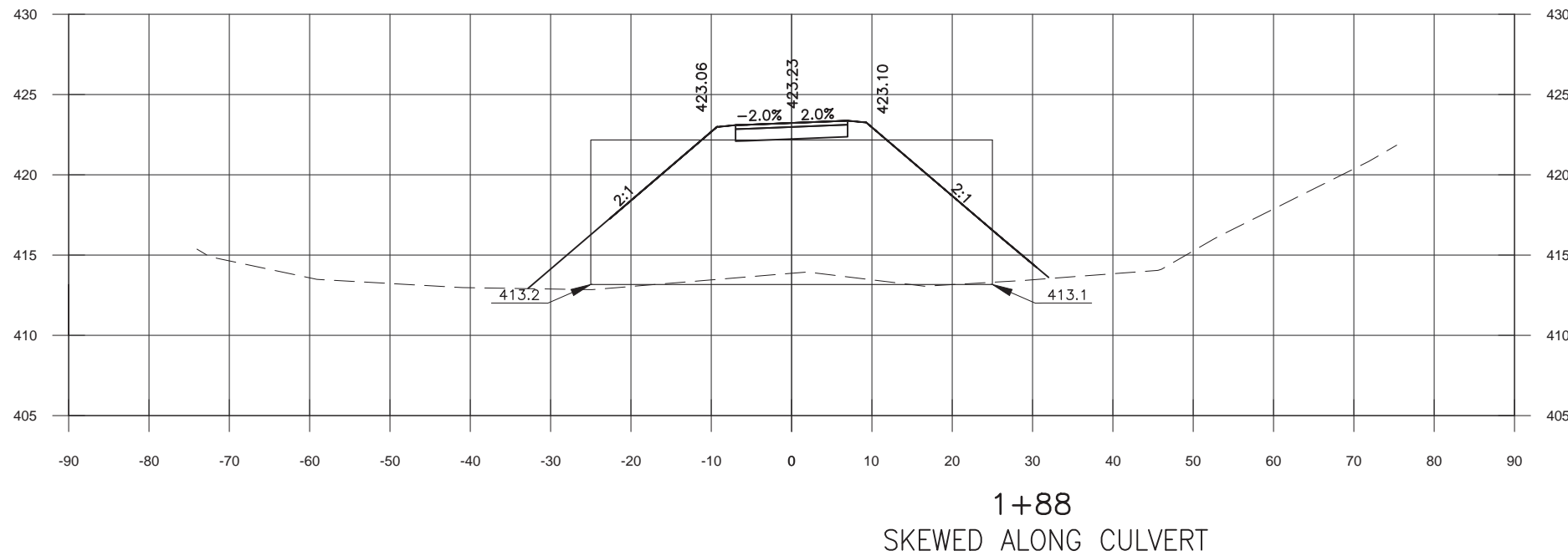
**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 1+00 TO  
 STA. 1+50**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS002
SHEET NO.





EARTHWORK STA. 2+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	0.00	178.38
FILL	263.54	161.34	580.65



EARTHWORK STA. 1+88.44			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	0.47	178.38
FILL	392.05	344.77	419.32

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED: DCL  
 DRAWN: DCL  
 CHECKED: JWS  
 APPROVED: SJK  
 DATE: 08-04-22

DATE: 08-04-22

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 Belleville, IL 62220

WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226

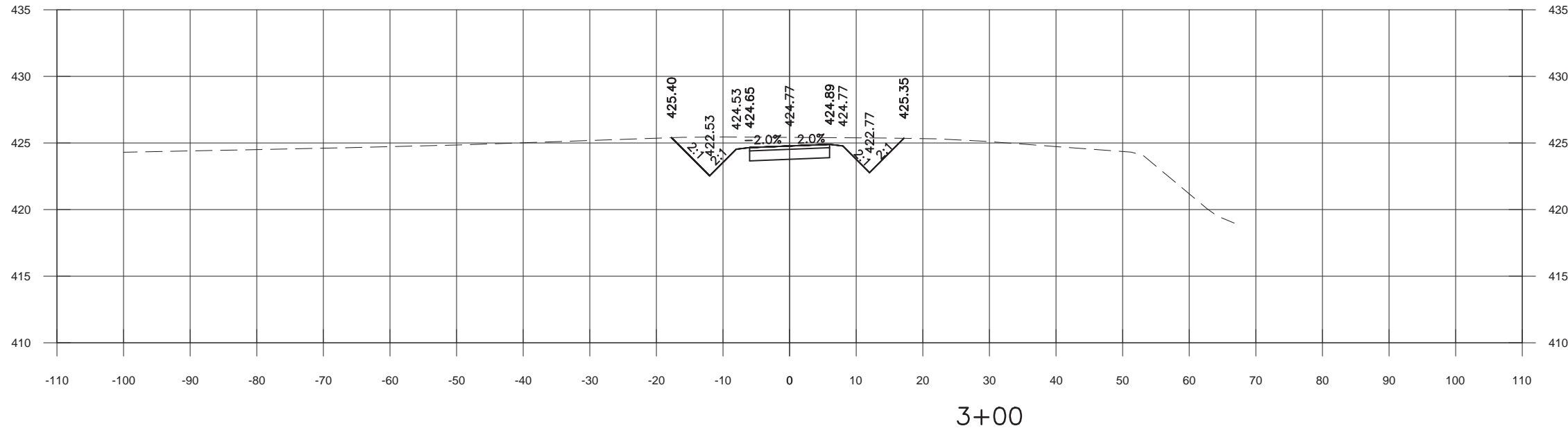


ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

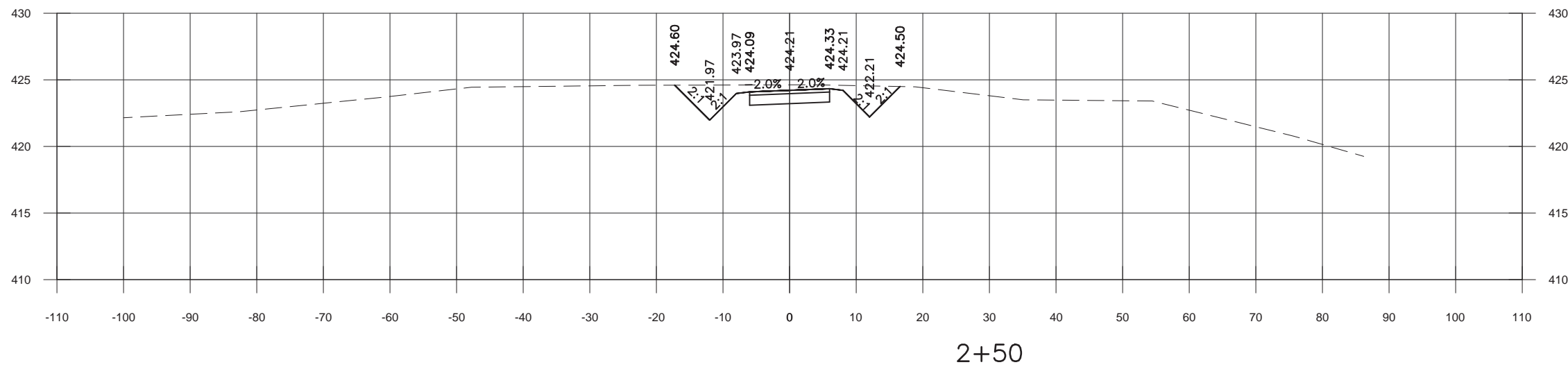
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 CROSS SECTIONS  
 STA. 1+88 TO  
 STA. 2+00**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS003
SHEET NO.





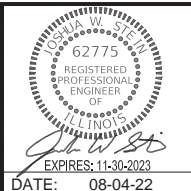
EARTHWORK STA. 3+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	51.72	87.61	305.73
FILL	0.00	0.00	861.28



EARTHWORK STA. 2+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	42.91	39.73	218.11
FILL	0.00	280.63	861.28

REV	DATE	BY	APP.	DESCRIPTION

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 DRAWN: DCL  
 CHECKED: JWS  
 APPROVED: SJK  
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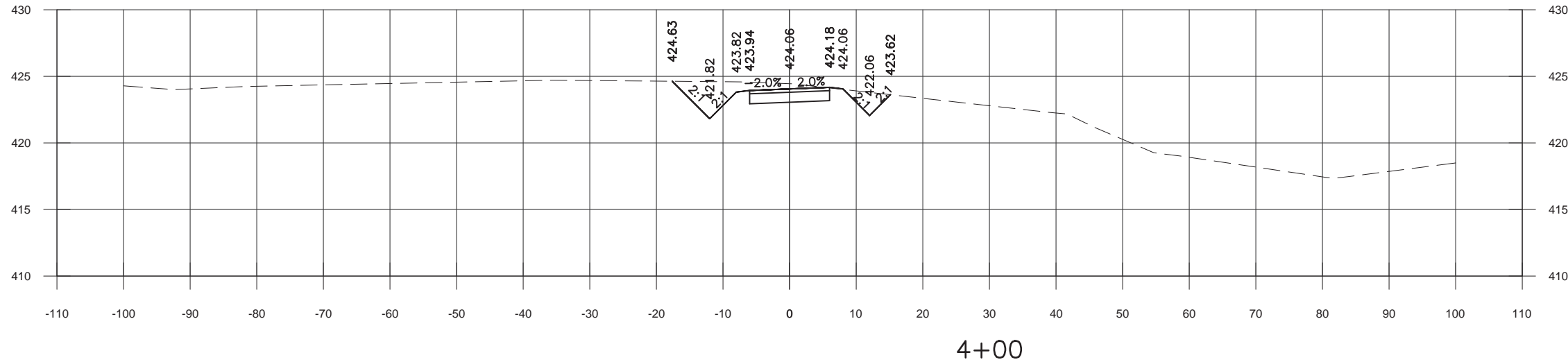
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



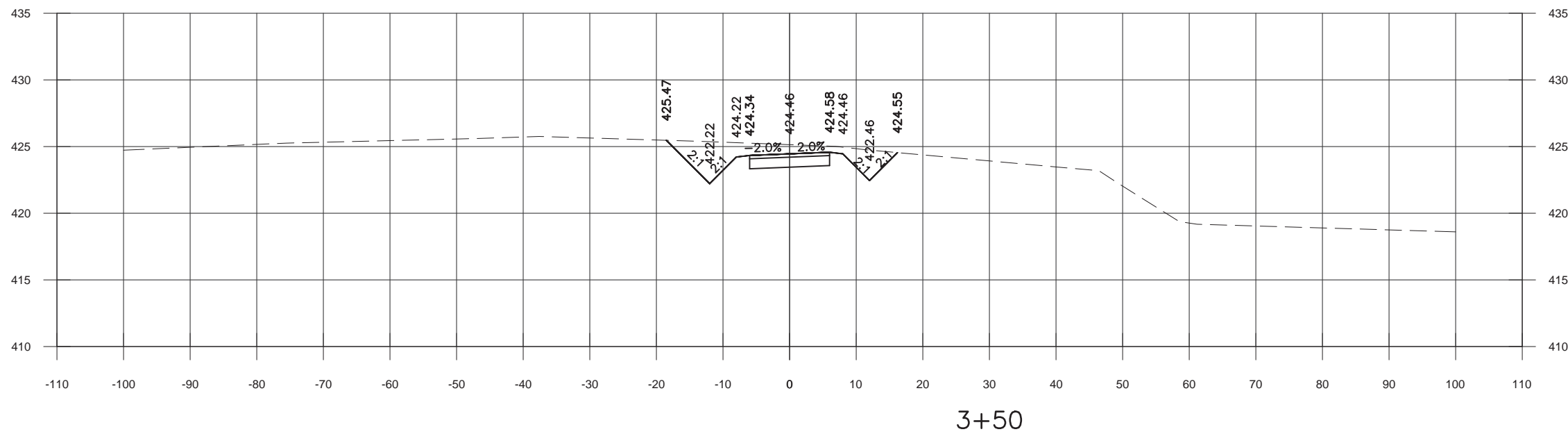
ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 2+50 TO  
 STA. 3+00**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS004  
 SHEET NO.



EARTHWORK STA. 4+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	38.61	84.15	486.16
FILL	0.09	0.09	861.37



EARTHWORK STA. 3+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	52.27	96.28	402.01
FILL	0.00	0.00	861.28

REV	DATE	BY	APP.	DESCRIPTION

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 CHECKED: JWS  
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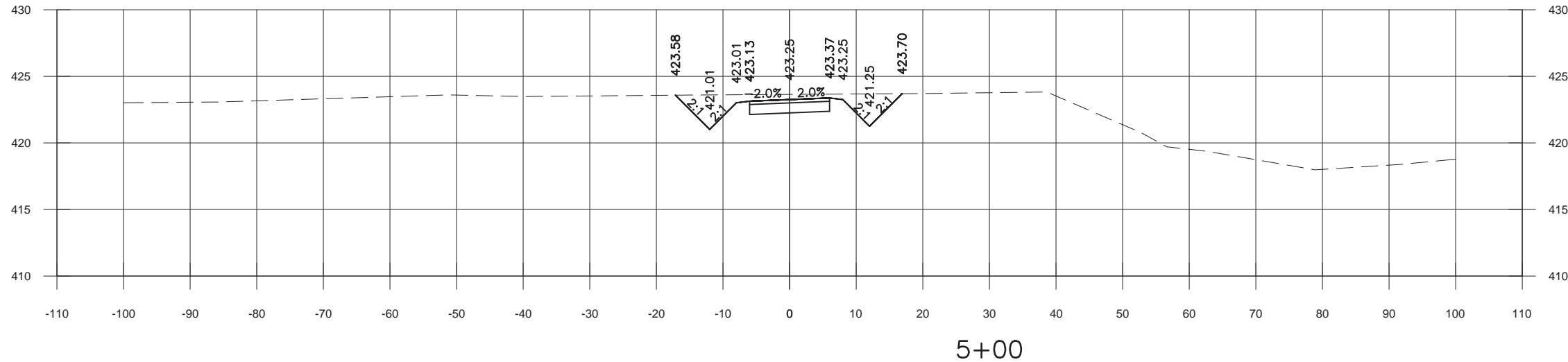
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



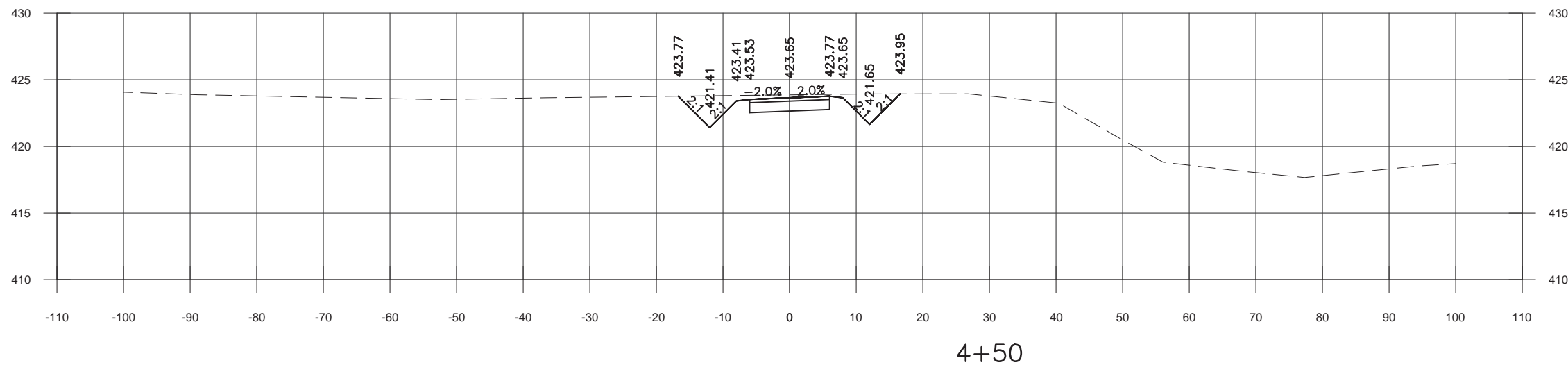
ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 3+50 TO  
 STA. 4+00**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS005  
 SHEET NO.



EARTHWORK STA. 5+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	43.13	74.53	630.98
FILL	0.00	0.00	861.46



EARTHWORK STA. 4+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	37.32	70.30	556.46
FILL	0.00	0.09	861.46

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED:	DCL
DRAWN:	DCL
CHECKED:	JWS
APPROVED:	SJK
DATE:	08-04-22

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 525 W. Main St., Ste. 125  
 Belleville, IL 62220

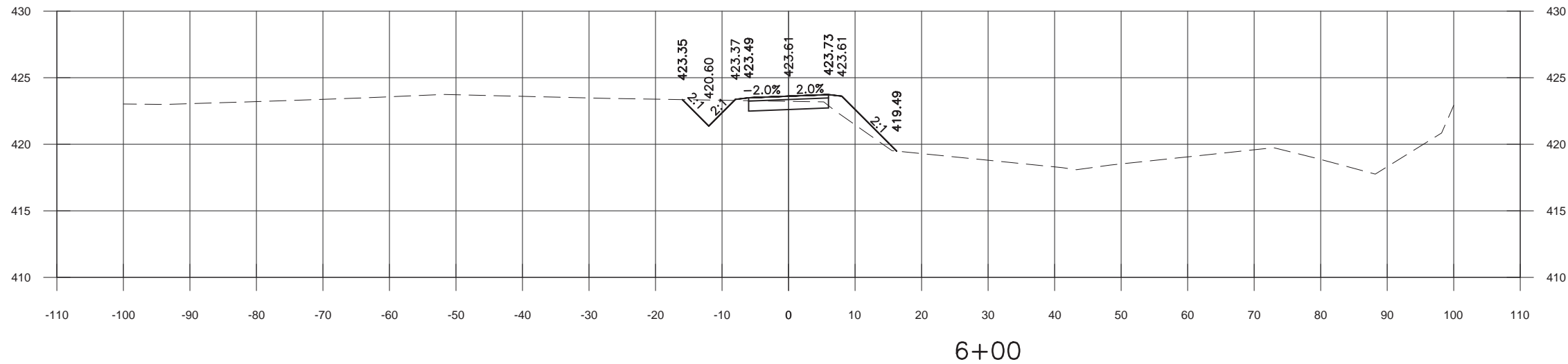
**wsp** WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226

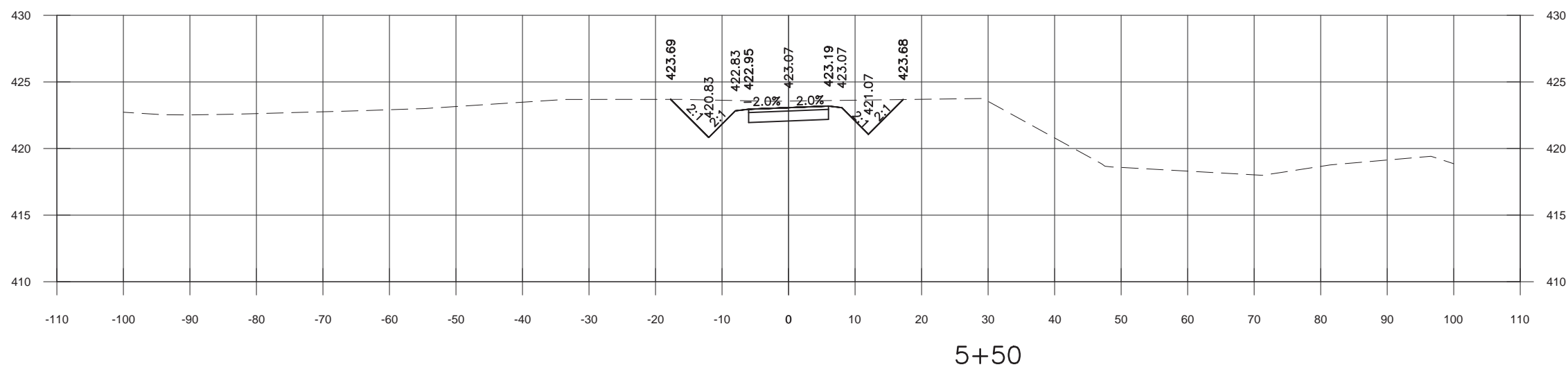


**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 4+50 TO  
 STA. 5+00**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS006
SHEET NO.



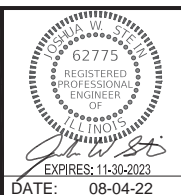
EARTHWORK STA. 6+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	14.92	59.01	775.11
FILL	9.38	9.83	871.29



EARTHWORK STA. 5+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	48.60	85.11	716.10
FILL	0.00	0.00	861.46

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED: DCL  
 DRAWN: DCL  
 CHECKED: JWS  
 APPROVED: SJK  
 DATE: 08-04-22



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 525 W. Main St., Ste. 125  
 Belleville, IL 62220

**wsp** WSP USA  
 211 N. Broadway, Ste. 2800  
 St. Louis, MO. 63102

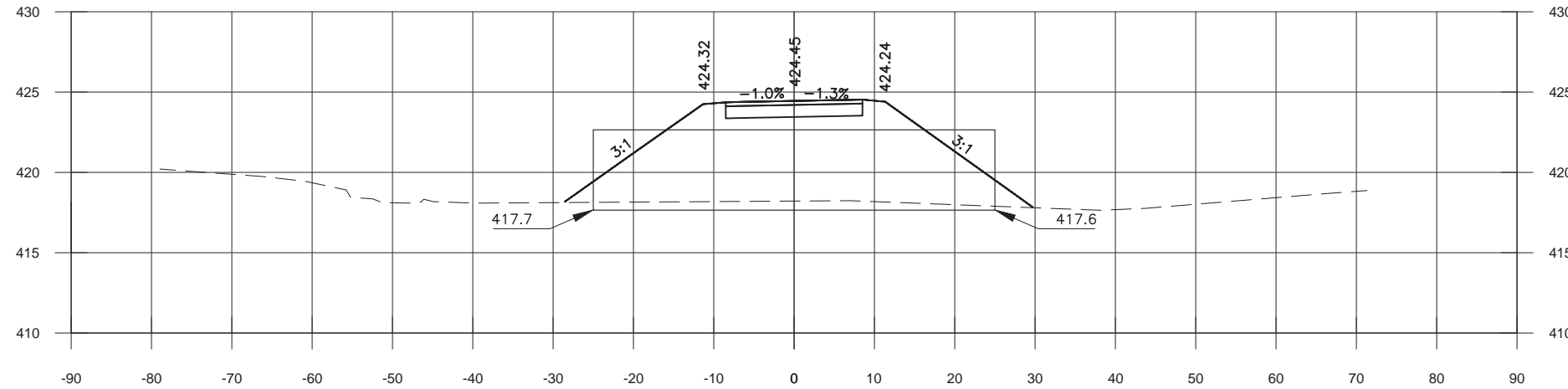
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

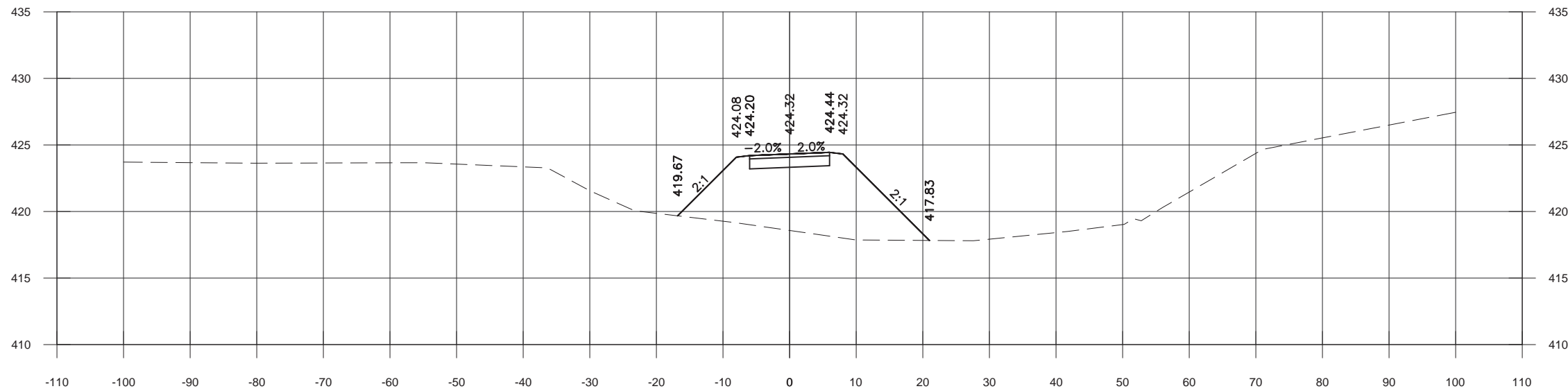
**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 5+50 TO  
 STA. 6+00**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS007
SHEET NO.



EARTHWORK STA. 6+70.04			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	0.00	788.92
FILL	234.62	161.25	1194.97

6+70  
SKEWED ALONG CULVERT



EARTHWORK STA. 6+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	13.81	788.92
FILL	143.16	162.42	1033.72

6+50

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED:	DCL
DRAWN:	DCL
CHECKED:	JWS
APPROVED:	SJK
DATE:	08-04-22

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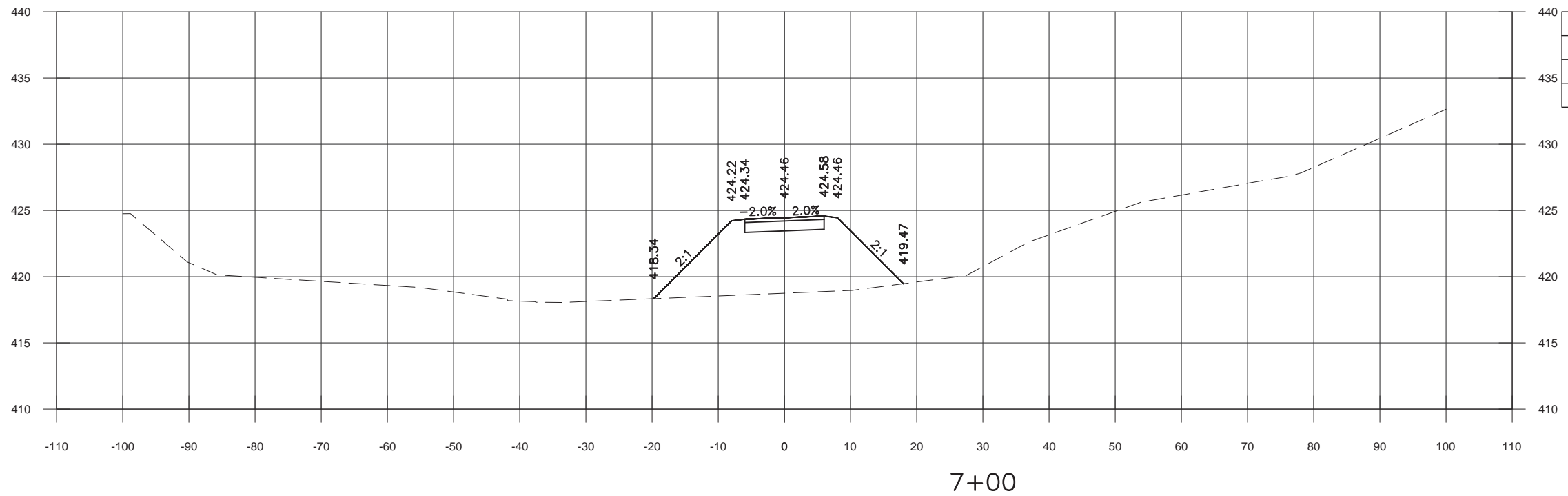
**TWM** TWM, Inc.  
4940 Old Collinsville Road  
Swansea, IL. 62226



ST. CLAIR COUNTY  
METROLINK EXTENSION  
SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
CROSS SECTIONS  
STA. 6+50 TO  
STA. 6+70**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS008
SHEET NO.



EARTHWORK STA. 7+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	0.00	788.92
FILL	140.11	239.07	1434.04

REV	DATE	BY	APP.	DESCRIPTION

DESIGNED: DCL  
 DRAWN: DCL  
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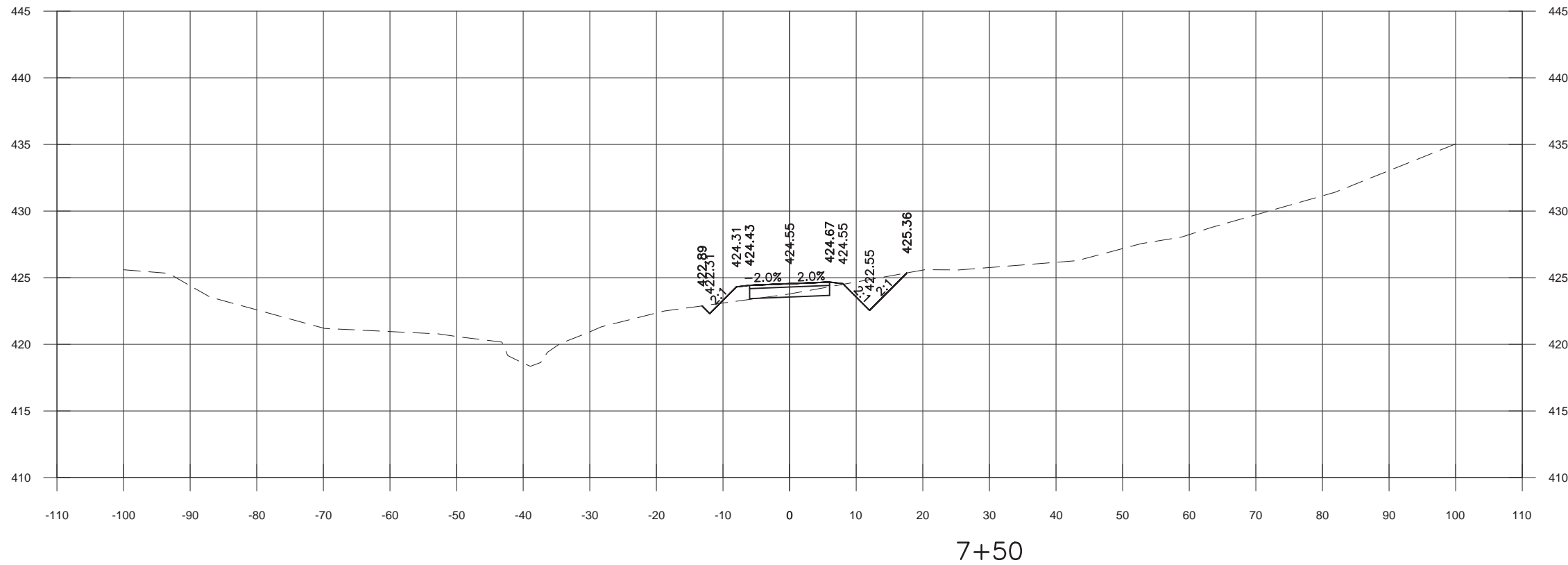
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
 Swansea, IL. 62226



ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 7+00**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS009  
 SHEET NO.



EARTHWORK STA. 7+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	15.03	13.91	802.84
FILL	3.92	153.37	1587.41

REV	DATE	BY	APP.	DESCRIPTION

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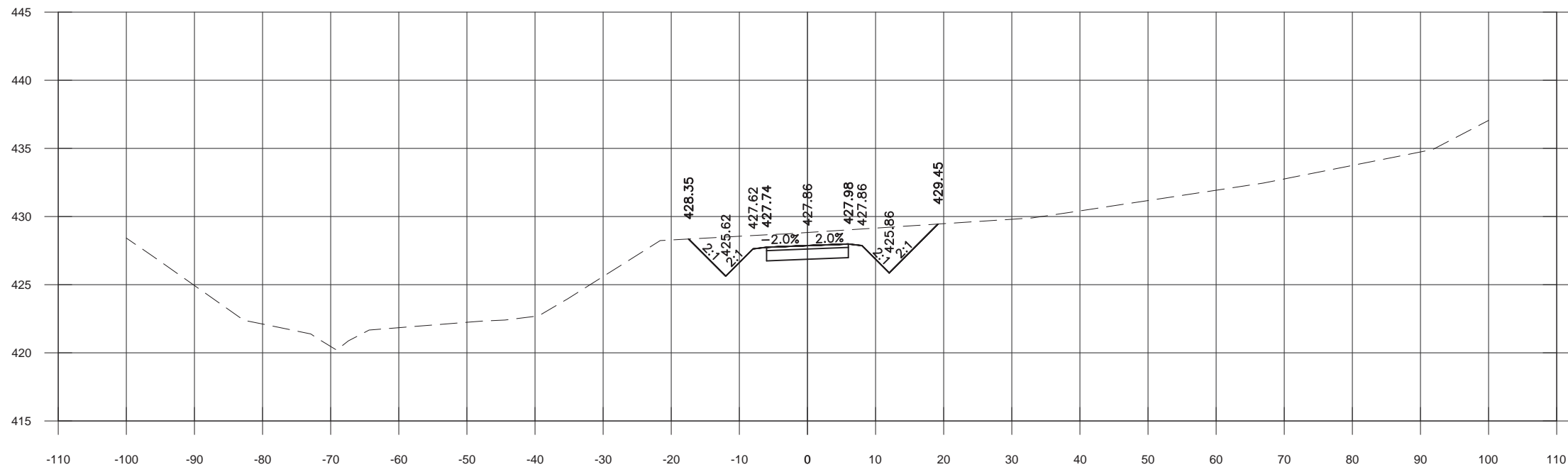
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
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ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

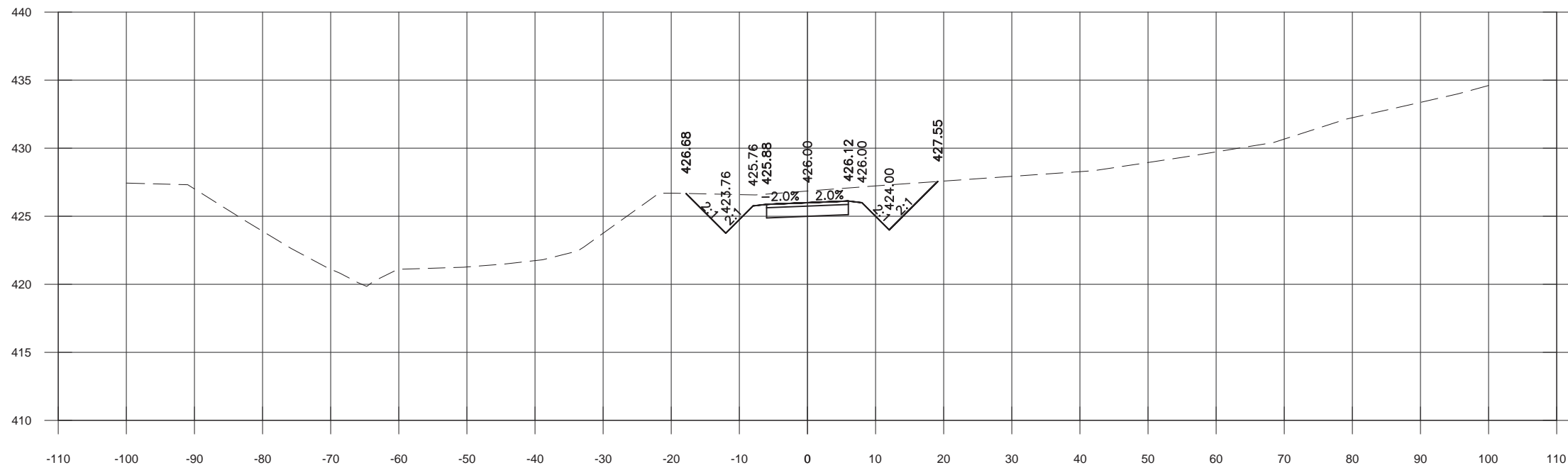
**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 7+50**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS010  
 SHEET NO.



EARTHWORK STA. 8+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	64.46	117.40	991.86
FILL	0.00	0.00	1591.59

8+50

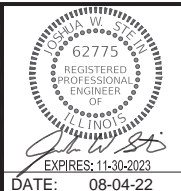


EARTHWORK STA. 8+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	62.33	71.62	874.46
FILL	0.00	4.18	1591.59

8+00

REV	DATE	BY	APP.	DESCRIPTION

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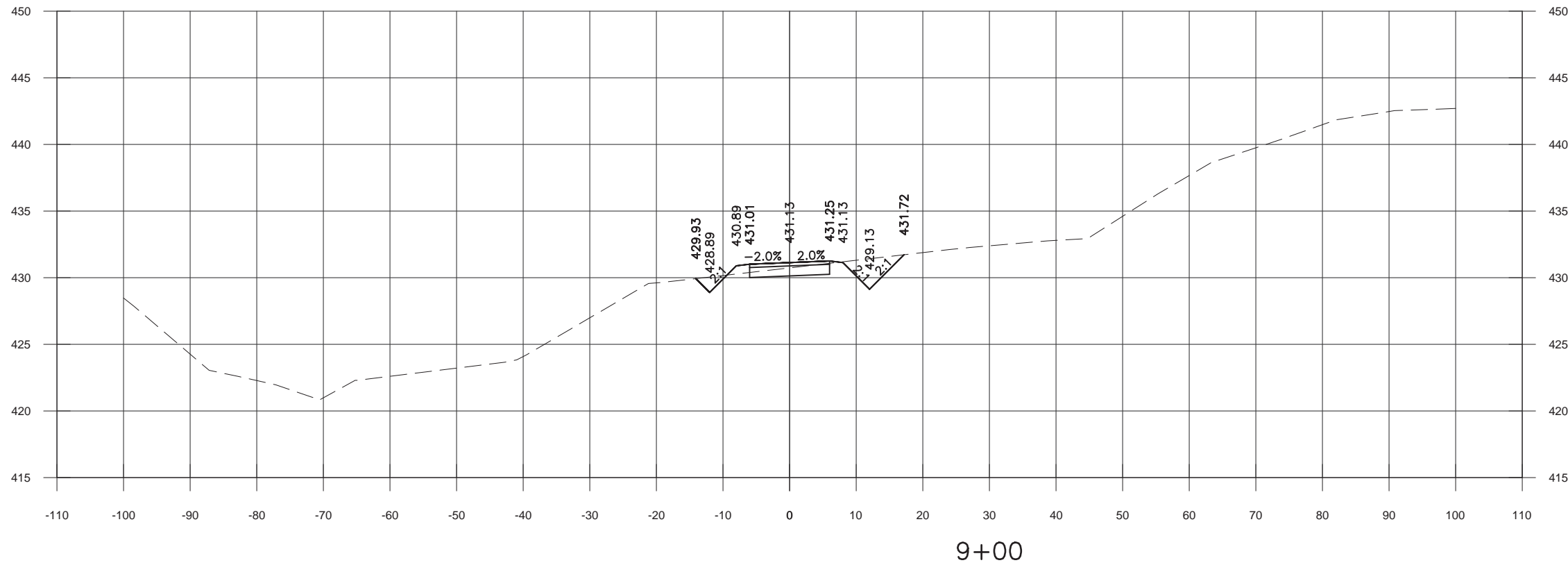


ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 8+00 TO  
 STA. 8+50**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. TxJKXS011
SHEET NO.





EARTHWORK STA. 9+00.00			
Material Name	Area	Volume	Cumulative Volume
CUT	20.57	78.74	1070.59
FILL	1.79	1.91	1593.50

9+00

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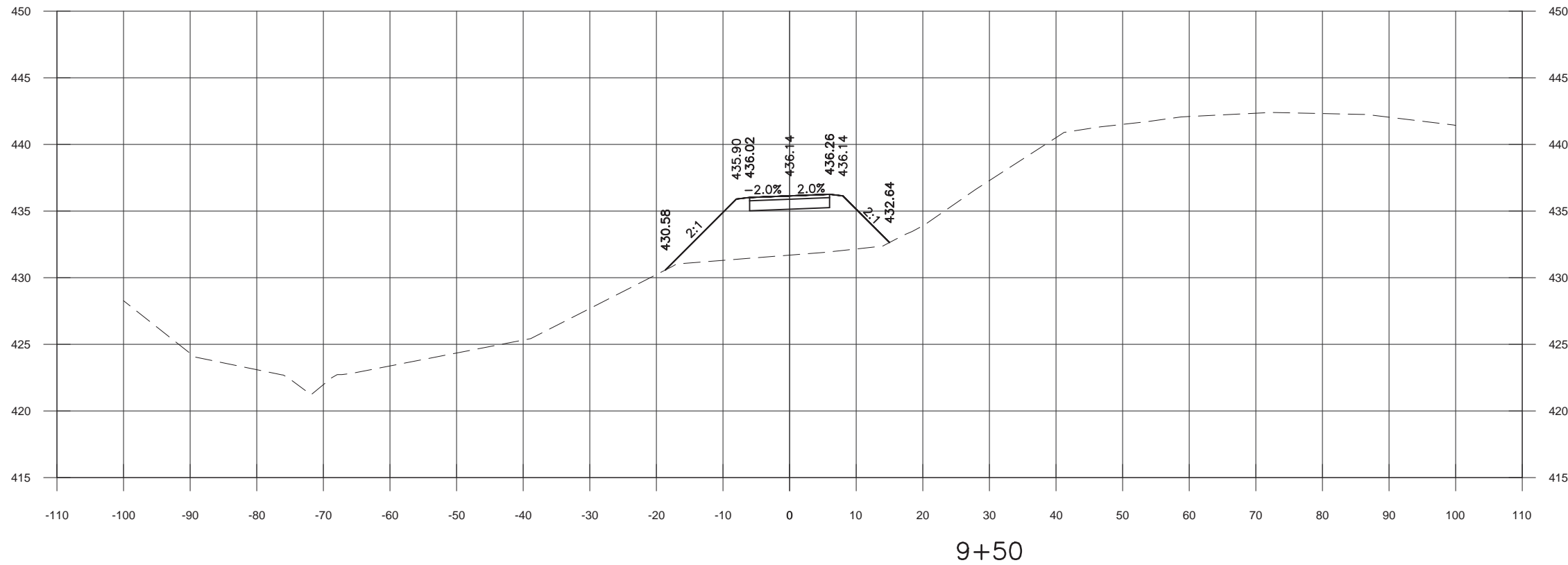
**TWM** TWM, Inc.  
 4940 Old Collinsville Road  
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ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 9+00**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS012  
 SHEET NO.



EARTHWORK STA. 9+50.00			
Material Name	Area	Volume	Cumulative Volume
CUT	0.00	19.05	1089.64
FILL	96.13	104.27	1697.77

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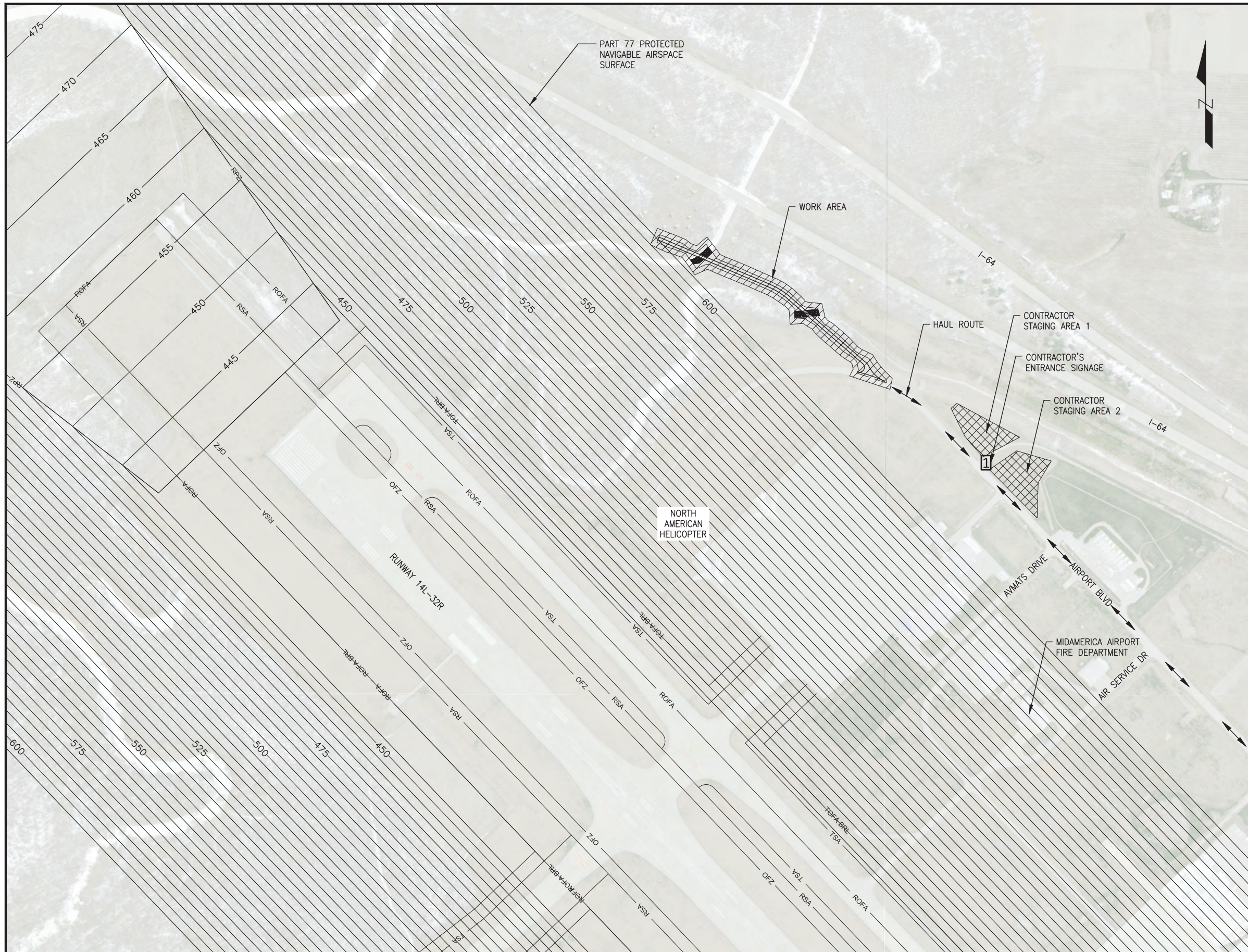


ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT





**AMEREN WATER X-ING  
 CROSS SECTIONS  
 STA. 9+50**

AMEREN WATER CROSSING  
 CONTRACT NO.  
 DRAWING NO. TxJKXS013  
 SHEET NO.





**LEGEND:**

-  CONTRACTOR STAGING AREA
-  WORK AREA
-  HAUL ROUTE
-  CONTRACTOR'S ENTRANCE SIGNAGE

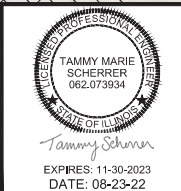
**GENERAL NOTES:**

1. CONTRACTOR SHALL COMPLETE WORK AS SHOWN IN THE CONTRACT DOCUMENTS.
2. THE INTERSECTION OF IL ROUTE 4 AND AIRPORT BOULEVARD IS THE ONLY POINT OF EGRESS FOR THE AIRPORT. THE INTERSECTION MUST REMAIN OPEN TO VEHICLE TRAFFIC AT ALL TIMES.
3. CONTRACTOR SHALL NOT ENTER THE AIR OPERATIONS AREA (AOA).
4. IF ACCESS OR HAUL ROUTES ARE DAMAGED DURING CONSTRUCTION, THE CONTRACTOR, UPON COMPLETION OF CONSTRUCTION, SHALL RESTORE THE ROUTE TO ITS ORIGINAL CONDITION AT THEIR COST.
5. IF FARM FIELDS ARE DAMAGED DURING CONSTRUCTION, THE CONTRACTOR, UPON COMPLETION OF CONSTRUCTION, SHALL RESTORE THE FIELDS TO ITS ORIGINAL CONDITION AT THEIR COST.
6. CONTRACTOR EQUIPMENT HEIGHT SHALL NOT EXCEED 30 FEET WITHIN THE IDENTIFIED WORK AREA UNLESS OTHERWISE APPROVED BY THE AIRPORT AND/OR ENGINEER. CONSTRUCTION EQUIPMENT MUST BE FULLY LOWERED PRIOR TO EXITING THE WORK AREA AND MUST REMAIN FULLY LOWERED INCLUDING WITHIN THE CONTRACTOR STAGING AREA.
7. THE CONTRACTOR SHALL NOT BE PERMITTED TO OPERATE OR PLACE EQUIPMENT OR MATERIALS OUTSIDE SPECIFIC WORK AREAS, HAUL ROUTES OR CONTRACTOR STAGING AREA. STOCKPILING OF MATERIALS IS NOT ALLOWED IN WORK AREAS. STOCKPILING IS LIMITED TO THE CONTRACTOR STAGING AREA. HEIGHT OF EQUIPMENT OR MATERIALS IN THE STAGING AREA ARE LIMITED TO 30 FEET.
8. CONTRACTOR SHALL MAINTAIN PAVEMENT CLEANLINESS BY SWEEPING AND/OR VACUUMING ON A DAILY BASIS OR MORE FREQUENTLY AS INSTRUCTED BY THE AIRPORT AND/OR ENGINEER.
9. CONTRACTOR SHALL MAINTAIN PAVEMENTS CLEAR OF MUD, DIRT, DEBRIS, ROCK, ETC FOR AIRPORT BOULEVARD, THE AIRPORT ENTRANCE AND IL ROUTE 4. CONTRACTOR SHALL SWEEP OR VACUUM WHEN INSTRUCTED BY THE AIRPORT AND/OR ENGINEER WHEN CONSTRUCTION VEHICLES EXIT THE HAUL ROUTE ONTO AIRPORT BOULEVARD.
10. CONTOURS REPRESENT THE 14 CFR PART 77 PROTECTED NAVIGABLE AIRSPACE SURFACE FOR BLV. ANY TEMPORARY, PERMANENT EQUIPMENT OR STRUCTURES THAT PENETRATE THIS SURFACE ARE CONSIDERED OBSTRUCTIONS TO THE NAVIGABLE AIRSPACE. CONTRACTOR EQUIPMENT SHALL NOT PENETRATE THE PART 77 SURFACE AT ANY TIME.
11. THE PART 77 MAXIMUM HORIZONTAL SURFACE ELEVATION IS 609.10 AND EXTENDS OVER THE PROJECT AREA.



REV	DATE	BY	APP.	DESCRIPTION

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DRAWN:	TS
CHECKED:	KR
APPROVED:	TS
DATE:	08-23-22



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ST. CLAIR COUNTY  
 METROLINK EXTENSION  
 SHILOH-SCOTT TO MIDAMERICA ST. LOUIS AIRPORT

**AMEREN WATER X-ING  
 CONSTRUCTION SAFETY  
 AND PHASING PLAN  
 SHEET 1 OF 3**

AMEREN WATER CROSSING
CONTRACT NO.
DRAWING NO. WxGWAV01
SHEET NO.





PENALTIES

1. IN THE EVENT AN EMPLOYEE OF THE CONTRACTOR VIOLATES A SAFETY PROVISION, THEY SHALL BE PROHIBITED FROM RETURNING TO WORK WITHOUT THE APPROVAL OF THE AIRPORT. VIOLATIONS MAY BE DEEMED AS JUST AND SUFFICIENT CAUSE TO REQUIRE THE EMPLOYEE BE PERMANENTLY REMOVED FROM THE JOB SITE. FOR THE CONTRACTOR'S VIOLATION OF THE SECURITY IDENTIFICATION DISPLAY AREA (SIDA), THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PENALTIES ISSUED BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) OR FAA.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS AND DELAYS CAUSED BY SAFETY VIOLATIONS. CONSTRUCTION PERSONNEL DRIVING ERRATICALLY ON THE AIRPORT, EXCEEDING THE 10 MPH SPEED LIMIT, OR VIOLATING ANY OTHER AIRPORT DRIVING RULE OR SAFETY REGULATION, AT A MINIMUM, SHALL BE REMOVED FROM THE PROJECT PERMANENTLY. AIRPORT OPERATIONS CAN REMOVE ANY CONTRACTOR PERSONNEL, AT ANY TIME, FOR ANY DURATION, DUE TO A SAFETY VIOLATION. AIRPORT OPERATIONS SHALL REPORT ANY OCCURRENCES TO THE CONTRACTOR AND THE OWNER.
3. NON-COMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE APPROVED CSPP AND SPCD, AIRPORT SECURITY REGULATIONS, VEHICLE SPEED RESTRICTIONS AND ANY OTHER APPLICABLE AIRPORT REGULATION MAY RESULT IN FINES AS ALLOWED BY LAW AS WELL AS THE REMOVAL OF THE CONTRACTOR PERSONNEL FROM THE WORK SITE.

SPECIAL CONDITIONS

1. EMERGENCY SITUATIONS: SHOULD A LIFE THREATENING INJURY OCCUR ON SITE, WHETHER IT BE TO THE CONTRACTOR'S WORKFORCE OR OWNER'S INSPECTION CREW OR OTHERS, THE OWNER SHALL BE IMMEDIATELY NOTIFIED AND THE APPROPRIATE AUTHORITIES. THE CONTRACTOR SHALL FOLLOW ALL INSTRUCTIONS BY THE OWNER, AIRPORT OPERATIONS AND AIRPORT/FAA OFFICIALS OR THEIR DESIGNEES.
2. THE CONTRACTOR SHALL YIELD TO ALL SAFETY PERSONNEL. IN THE EVENT OF A WORK STOPPAGE DUE TO AIRCRAFT EMERGENCY, THE CONTRACTOR SHALL, IF POSSIBLE, MAKE SAFE THE CONSTRUCTION SITE AND VACATE THE AREA UNDER ESCORT.
3. THE CONTRACTOR SHALL TAKE FULL RESPONSIBILITY OF THE SAFETY OF WORKERS THROUGHOUT THE CONSTRUCTION SITE AND FOR THE ENTIRE DURATION OF THE PROJECT.

RUNWAY AND TAXIWAY VISUAL AIDS

1. NO RUNWAY CLOSURES ARE ANTICIPATED FOR THIS PROJECT.

HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING IN-KIND ANY MARKINGS DESTROYED, OBLITERATED, OR OTHERWISE RENDERED UNUSABLE BY CONSTRUCTION ACTIVITIES, UNLESS OTHERWISE NOTED IN THE PLANS. CONTRACTOR SHALL ENSURE CONTINUITY OF SERVICE FOR LIGHTING SYSTEMS NOT ASSOCIATED WITH THIS PROJECT.
2. ALL CONSTRUCTION VEHICLES AND EQUIPMENT SHALL HAVE A FLASHING YELLOW BEACON AND/OR 3 FEET BY 3 FEET CHECKERED ORANGE AND WHITE FLAG ATTACHED TO VEHICLES/EQUIPMENT (SUCH AS CRANES) AS SPECIFIED IN FAA ADVISORY CIRCULAR 150/5210-5D "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT". EACH VEHICLE SHALL HAVE THE CONTRACTOR'S NAME CLEARLY IDENTIFIED ON BOTH SIDES OF THE VEHICLE.

WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

1. CONTRACTOR SHALL DEVELOP SIGNAGE AND LIGHTING PLAN FOR NIGHTTIME WORK THAT WILL BE SUBMITTED TO THE AIRPORT AND/OR ENGINEER FOR APPROVAL WITH A MINIMUM OF 5 DAYS PRIOR TO COMMENCING NIGHTTIME WORK.
2. LIGHTING EQUIPMENT MUST ADEQUATELY ILLUMINATE THE WORK AREA IF CONSTRUCTION IS TO BE PERFORMED DURING NIGHTTIME HOURS. IT IS RECOMMENDED THAT ALL SUPPORT EQUIPMENT, EXCEPT HAUL TRUCKS, BE EQUIPPED WITH ARTIFICIAL ILLUMINATION TO SAFELY ILLUMINATE THE AREA IMMEDIATELY SURROUNDING THEIR WORK AREAS.
3. LIGHT SHOULD BE POSITIONED TO PROVIDE THE MOST NATURAL COLOR ILLUMINATION AND CONTRAST WITH A MINIMUM OF SHADOWS. THE SPACING MUST BE DETERMINED BY TRIAL. LIGHT TOWERS SHOULD BE POSITIONED AND ADJUSTED TO AIM AWAY FROM THE ATCT CAB AND ACTIVE RUNWAYS TO PREVENT BLINDING EFFECTS. SHIELDING MAY BE NECESSARY.
4. NIGHTTIME WORK IS NOT ANTICIPATED FOR THIS PROJECT.

PROTECTION OF RUNWAY, TAXIWAY SAFETY AREAS AND OBJECT FREE AREAS

RUNWAY SAFETY AREA (RSA)

1. THERE SHALL BE NO WORK WITHIN ANY ACTIVE RSA.

RUNWAY OBJECT FREE AREA (ROFA)

1. THERE SHALL BE NO WORK WITHIN ANY ACTIVE ROFA.

TAXIWAY SAFETY AREA (TSA)

1. THERE SHALL BE NO WORK WITHIN ANY ACTIVE TSA.

OBSTACLE FREE ZONES

1. THERE SHALL BE NO WORK WITHIN ANY ACTIVE OBSTACLE FREE ZONES (OFZ).

RUNWAY APPROACH/DEPARTURE SURFACES

1. THERE SHALL BE NO WORK WITHIN ANY ACTIVE RUNWAY APPROACH/DEPARTURE AREAS OR CLEARWAYS.

OTHER LIMITATIONS ON CONSTRUCTION

THE FOLLOWING RESTRICTIONS WILL BE IN PLACE DURING THE CONSTRUCTION OF THIS PROJECT UNLESS OTHERWISE ACCEPTED BY WRITTEN AUTHORIZATION:

- NO USE OF TALL EQUIPMENT (CRANES, CONCRETE PUMPS, AND SO ON) UNLESS A 7460-1 DETERMINATION LETTER IS ISSUED FOR SUCH EQUIPMENT.
- NO USE OF OPEN FLAMES, WELDING, TORCHES OR BURN PITS UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED AND APPROVED IN WRITING BY THE AIRPORT
- NO USE OF FLARE POTS WITHIN THE AOA AT ANY TIME.
- NO USE OF ELECTRICAL BLASTING CAPS ON OR WITHIN 1,000 FEET OF AIRPORT PROPERTY.
- NO SMOKING WITHIN THE AIRPORT FACILITIES OR WITHIN 20 FEET OF ENTRANCES, OPERABLE WINDOWS, OR OUTDOOR AIR INTAKES.
- NO DISRUPTION OF UTILITIES SERVING THE FACILITIES OCCUPIED BY THE AIRPORT OR THEIR TENANTS, UNLESS PERMITTED IN WRITING (WITH PROVISIONS FOR TEMPORARY UTILITIES IN THEIR PLACE). THE CONTRACTOR SHALL NOTIFY THE AIRPORT AND OWNER A MINIMUM OF 48 HOURS IN ADVANCE OF PROPOSED UTILITY DISRUPTIONS AND SHALL NOT PROCEED WITH UTILITY INTERRUPTION WITHOUT WRITTEN PERMISSION.

36"

**CONTRACTOR'S  
NAME / PROJECT  
VEHICLE /  
EQUIPMENT  
PARKING ONLY  
NO PUBLIC ACCESS**

24"

CONTRACTOR'S ENTRANCE SIGNAGE DETAIL

SCALE: NTS

1. CONTRACTOR SHALL PLACE THE SIGN AT THE ENTRANCE TO THE CONTRACTOR'S STAGING AREA, FACING THE STAGING AREA ENTRANCE, AS SHOWN ON SHEET 1 OF THE CSPP.
2. CONTRACTOR SHALL MAKE THE SIGN WEATHER PROOF.
3. CONTRACTOR SHALL INSTALL THE SIGN AT LEAST 5- FEET ABOVE GROUND LEVEL.
4. CONTRACTOR SHALL ENSURE SIGNAGE REMAINS IN PLACE THROUGHOUT THE DURATION OF THE CONTRACT.

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DRAWN:	TS							CONTRACT NO.
CHECKED:	KR			DRAWING NO.	WxGKAV03			
APPROVED:	TS			SHEET NO.				
REV	DATE	BY	APP.	DESCRIPTION	DATE:	08-23-22		